

Figure 1: Potential hamlet boundary

1.3 History of Noyac¹

Noyac has a distinctive historic character, comprised of cultural and historic structures and archaeological sites, as well as unique natural resources, which contribute to the scenic quality of the area. The landscape of Noyac reflects a perfect union between the historical built environment, the hamlet’s natural beauty, and a working landscape clearly shaped by past generations. Historic landscapes and landmarks are integral elements of this union that distinguishes Noyac from other places and creates an unparalleled sense of community. Noyac’s history and historic places are not only an important aspect of the community’s past but of its future. These resources provide an integrity of place; they are a valuable community asset.

The history of Noyac stretches from the life and times of the Native Americans -- before the arrival of the first white settlers to Noyac in 1679 -- through the Revolution, the Civil War and the suburbanization of the late 20th century. The following provides an overview of the historic trajectory.

First Settlers

For several thousand years, there were probably a considerable number of Native American village sites in and around Noyac. These settlements were clustered around the fresh water streams and springs that flowed north out of the moraine and into Peconic Bay. The fin and shellfish around the bay provided abundant food resources. Native American artifacts and evidence of prehistoric occupation have been found along the bay shoreline.

Colonial Settlers

The English colonists who settled in Southampton in 1640 did not divide the lands of Noyac until 1679. John Jessup acquired in that

¹ Information for this section of the report was excerpted from the *Noyac Hamlet Heritage Resources Report* approved September 16, 2003.

- The Noyac Community Advisory Committee (CAC)- established by the Town as a sounding board for policy issues within the hamlet, represented by members from within the Town-approved hamlet boundary (hence the importance of the previously-discussed boundary issue).
- The Noyac Civic Council, Inc. - a volunteer group that undertakes service projects for the hamlet, with a broader geographic base.
- Noyac Road business leaders - not an organization, but a category representing primarily merchants along Noyac Road.

Following the Committee meeting, the Committee and team conducted a kick-off meeting with the community to present project scope and elicit general comments and concerns (see **Figures 2 and 3**).



Figure 2: Consultant team members present to general public at Kick-off Meeting on May 1, 2003.

Task 2: Focus Group Meetings

The Consultant team conducted focus group meetings with a variety of recommended stakeholders and other participants to understand project issues and the overall strategic and physical context. These meetings took place on June 17, 2003, and included group discussions with the Noyac CAC, the Noyac Civic Council, Inc., various merchants, representatives of individual neighborhood groups, Town trustees, and Town planning and engineering staff and fire/ police personnel.



Figure 3: Over fifty Noyac residents attended the Kick-off Meeting

Task 3: Analysis of Area Problems and Opportunities

The Consultant team conducted a review of issues and opportunities as well as detailed field analysis of site conditions in conjunction with Town and community representatives. The focus was on traffic circulation/ traffic-calming, access/ egress, open space enhancement, urban design/landscape architecture, zoning and community character.

- Driveways near curves
 - Sight distance at intersections
 - Specific “problem areas” along Noyac Road which exhibit the potential for safety problems:
1. Cromer’s Market Area--Existing parking for Cromer’s and the adjacent businesses is “head-in,” and continuous access is permitted along the north side of Noyac Road. This uncontrolled access from Noyac Road leads to multiple points of conflict between vehicles entering parking spaces, backing from the spaces, and proceeding through the area. In addition, vehicles (especially trucks) parking on the north side of Noyac Road restrict sight distance east of Cromer’s. Some control of access to parking is necessary to increase safety (see Figure 4). Bay Avenue and Elm Street intersect Noyac Road at the west end of this area. These intersections are immediately adjacent to one another, resulting in additional difficulties for motorists. Noyac Road also curves at this location and the roadway banking (superelevation) appears to be improper.

2. “Deli Area”-- During the day, much of the parking and pedestrian activity is related to The Pantry Delicatessen and the liquor store on the south side of the roadway. At times, particularly when trucks are parked on both shoulders, traffic flow on Noyac Road is affected. Pedestrian crossings from vehicles, which are parked on the shoulder/grass areas along the north side of the roadway, are a concern (see Figure 5).
3. Trout Pond--The horizontal “S” turn on Noyac Road, combined with a change in vertical grades in this area, is a potential safety problem.
4. Long Beach Road Intersection -- Motorists traveling south on Long Beach Road from North Haven experience delays in waiting for gaps in traffic to enter Noyac Road. The existing intersection configuration, which includes a “circle” for vehicles entering or exiting Long Beach Road, can be confusing to motorists.



Figure 4: View looking east on Noyac Road, east of Bay Avenue. The existing “head-in” parking negatively affects traffic flow and safety along Noyac Road.



Figure 5: Mid-day view looking east on Noyac Road, east of Pine Neck Avenue. Parking for the Pantry Deli, including trucks, occurs on both shoulders of the road.

side of the road and some landscaping, which could extend into the shoulder area. They can be supplemented with rumble strips.

- *Intersection sight distance survey.* The available sight distance along Noyac Road for motorists stopped on intersection approaches should be measured. This distance should first be optimized by clearing vegetation within the roadway right-of-way where required. If limited sight distance still exists, intersection-warning signs should be installed in advance of these locations for motorists approaching on Noyac Road.
- *Install crosswalks.* Wherever there are a significant number of pedestrians crossing the roadway, crosswalk installation should be considered. Sidewalks should either exist or be constructed at these locations. It would be desirable to construct sidewalk “bulb-outs” into the roadway shoulder area. An example is shown in **Figure 6**. This would enhance the visibility of the pedestrian to the motorist and shorten the crossing distance where the pedestrian is “exposed” to approaching motorists. Pedestrian crossing warning signs should be installed at these locations as well.

- *Provide shoulder pavement markings and delineators.* Shoulder pavement markings can be used to further delineate the shoulder area, and to assist in lowering speeds on curves. These markings consist of wide, diagonal white (“zebra”) stripes installed across the shoulder. On straight sections and on approaches to curves, these could be installed at intervals of 100 feet. Closer to the beginning of the curve, the spacing interval is gradually reduced. The motorist becomes accustomed to passing each stripe in a particular time interval, say every 2 seconds, on the straight section. When the interval is shortened, a subtle suggestion is given to the motorist that he is traveling too fast, because the stripes are being passed more quickly (the markings can also consist of a “chevron” shape, and be placed in the travel lane itself on approaches to curves (this technique is currently being used on the Cross Island Parkway exit from the Throgs Neck Bridge in Queens). The markings can be supplemented with delineators, consisting of a reflector on a plastic post around curves (see **Figure 7**).



Figure 6: Example of sidewalk “bulb-outs.”

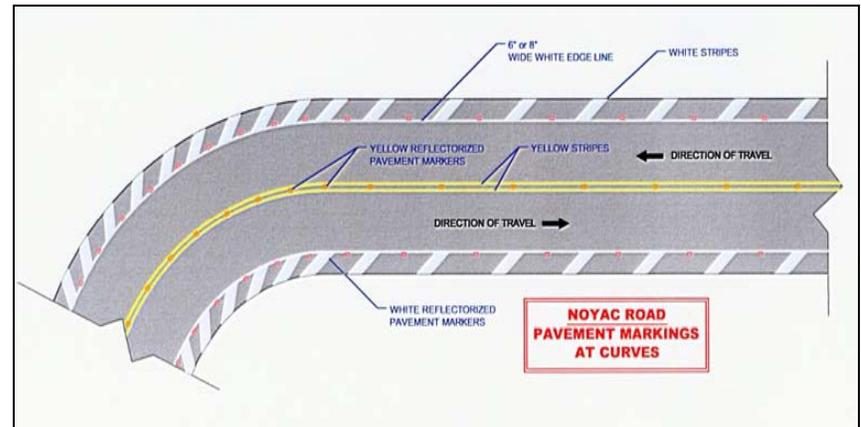


Figure 7: Pavement markings at curves.

- Evaluate the use of innovative signs. As noted previously, this portion of Noyac Road traverses an area that is largely residential. As a result, although the roadway functions as a “through” route, there are numerous residential driveways. This presents a potential safety problem for residents, particularly where driveways are located on curves in the roadway. To enhance a motorist’s perception of a situation which could occur when he enters a curve and is suddenly confronted with a vehicle backing into his lane, we have developed the sign shown in **Figure 8**. It is anticipated that this sign will be installed by the Town Highway Department, on a trial basis, at one or two locations on Noyac Road. Rather than create a proliferation of these signs, which might lead to motorists’ disregarding them, they should probably be placed in advance of areas with clusters of these driveways. Although there are several “Hidden Driveway” signs located along the entire length of Noyac Road, these are small and do not have the impact of the symbol sign.

The following recommendations have been developed for specific “problem areas” identified in Section 1.4 of this study. These recommendations are longer-term solutions because they require:

- field survey/property easements/development of plans
- additional permitting

The Town is already investigating funding sources to pursue the first four of these recommendations.

- *Cromer’s Market Area (see Section 2.2)* - Traffic movements at these intersections can be channelized, in conjunction with access to the parking area to improve both traffic flow and safety. Bay Avenue and Elm Street could be combined into one roadway approach to Noyac Road by realigning Elm Street to intersect Bay Avenue prior to its intersection with Noyac Road. Cedar Lane can be utilized for parking to replace spaces lost due to angling the spaces at the businesses and to accommodate spaces lost by prohibiting parking on the north side of Noyac Road east of Cedar Lane. Noyac Road would be realigned to smooth the horizontal curve and provide proper roadway banking, or super-elevation, around the curve. The Town owns much of the property along the south side of Noyac Road. Easements would be required from business owners, the property owner east of Cedar Lane on the north side and the property owner west of Town-owned property on the south side. Sketch plans of these improvements appear in Section 2.2.
- “Deli Area” - In conjunction with sidewalk construction to connect businesses in this area, crosswalk(s) should be established using the sidewalk “bulb-outs” and pedestrian warning signs discussed above. During the day, much of the parking and pedestrian activity is related to the delicatessen on the south side of the roadway. The roadway could possibly be widened to a minimum width of 37 feet to allow parking on both sides of the roadway at “high customer turnover”

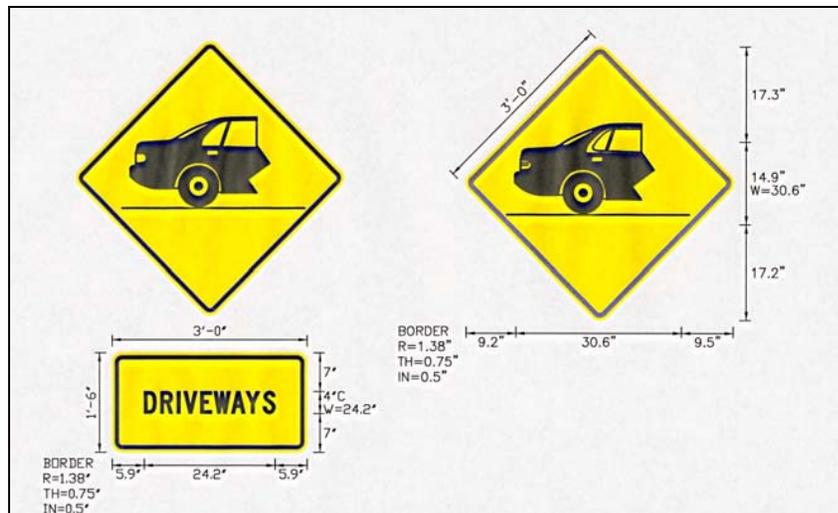


Figure 8: Longer-term solutions: signage to be installed along Noyac Road.

□ **From the “Deli” to Trout Pond**

This site, as described in the existing conditions section, is located on both the north and south sides of Noyac Road. From west to east, this key portion of Noyac Road contains a variety of uses: Trout Pond itself and its surrounding open space, the commercial area at the foot of the marina, the vacant ‘motel site’ opposite, various residential properties in what is now a Village Business zone, the Noyac Professional Center (an approved but not constructed new low-rise office development), and the various commercial buildings surrounding the Pantry Deli (see **Figure 9**).

Along with limited access to parking, this commercial node also lacks cohesiveness and connectedness between the uses. Further, given the proposed office center on the Burke property, questions remain as to how to link this development to existing commercial and retail uses.



Figure 9: View of “Deli” area looking west on Noyac Road.

More specifically, around this area a number of issues were raised:

- Lack of sufficient on-site parking leading to problems between adjacent stores;
- Poor parking configurations in front of stores, requiring backing into the busy road;
- Difficulty in crossing the roadway with speeding cars and heavy traffic;
- Lack of a central focus for the area and pedestrian connections between adjacent stores; and
- Fragmented existing development and the potential for inappropriate infill development.

Accordingly, this study generally recommends:

- ✓ Controlling access in front of the buildings with angled parking.
- ✓ Improving the use of the rear-yard to increase availability of on-site parking space.
- ✓ Amending zoning to achieve desired uses and site configurations.

Proposed Development Approaches

Commercial Development West of the “Deli”

The commercial area as a whole is zoned (VB), Village Business (see **Map 5**) and contains a number of vacant or underutilized sites that, according to the build-out analysis, might in the future be developed ‘as of right’ under existing zoning for commercial establishments of a larger size and configuration than desired by the community. The commercial area is interspersed with residentially zoned land, some subdivided into small lots but with substantial large tracts of undeveloped land, including the former ‘motel site,’ now undeveloped (see **Figure 10**).



Figure 10: Aerial photo of 'Motel Site' - existing residential zoning and potential venue for housing.

The proposed development approach is to rezone major parts of the undeveloped area from Village Business (VB) to Hamlet Office (HO) and Hamlet Commercial (HC), while at the same time proposing design patterns that can be encouraged for new development.

Applying the new HO and HC zones recently adopted by the Town will affect the western portion of the existing VB zone on both the north and south sides of Noyac Road (see **Figures 11 and 12**). It will maintain the existing concentrated retail node around the Deli, but will also ensure that new development to the west provides more office and small-scale commercial type uses and follows a more residentially scaled model.

A more detailed description of the results, including the use and design implications of these zoning recommendations, is found in Section 3.1 of this report.

The conceptual sketches (1-8) shown on the following pages illustrate potential scenarios for the development of several key locations within the Study Area. These include the commercial node surrounding 'the Deli,' the 'Motel Site' and Cromer's Market area. These sketch plans were developed by the Consultant with community input at the design charrette to depict proposed features and investigate alternative design elements to be implemented.

Sketch 1 describes the potential of this area, given the proposed zoning changes. Currently allowable development under the Village Business (VB) category could result in "as-of-right" large buildings and parking fields. This alternative approach, the new HC and HO districts, reduce that development to a less dense, more residentially scaled plan, more in keeping with the existing "look and feel" of the area as it is today.

In addition, recommended site planning/hamlet design approaches emphasize creation of a pedestrian-oriented area, with sidewalks linking the buildings in this district so that one can park one's car and walk to a variety of destinations without having to drive from one site to another. Parking would be behind buildings, not visible from the road and interconnected between parcels to allow parallel circulation and minimize curb cuts and access ways onto Noyac Road.

An important possible feature, with the participation of landowners who may develop the land for any purpose in accordance with applicable zoning regulations, is the provision of public amenities such as a new 'hamlet green' or roadside landscaping-- features that will add value to the properties on which they occur, to adjacent properties fronting the amenities, as well as the area as a whole.

Zoning Districts

-  HO
-  HC
-  OSC
-  R10
-  R15
-  R20
-  R40
-  R60
-  R80
-  RWB
-  VB

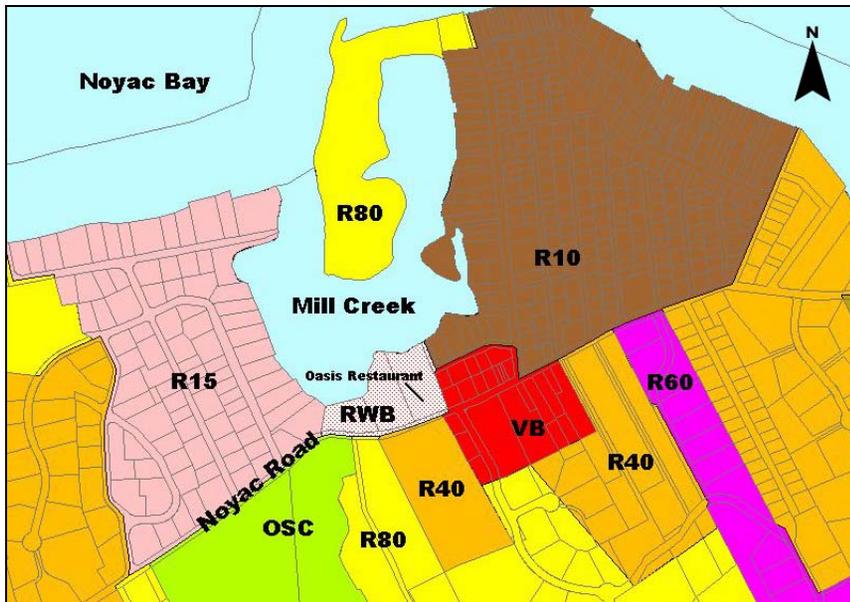


Figure 11: Existing zoning for area.

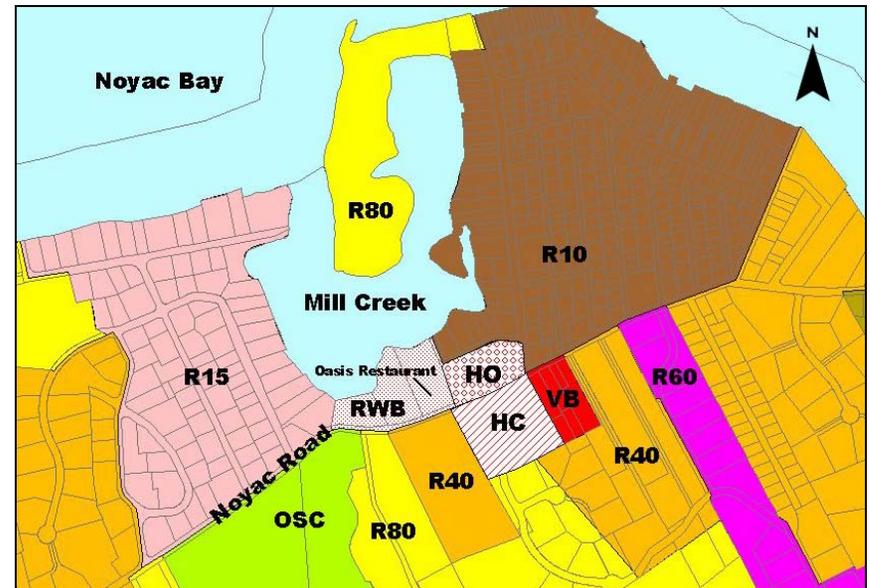


Figure 12: Proposed zoning: change western portion of VB zone to Hamlet Commercial (HC) on the south side of Noyac Road and Hamlet Office (HO) on the north side of Noyac Road.

leading to the rear of the property. This road extends as a paper right of way almost to the shore, although because of wetlands it can never be completed as a real accessway. Trading a portion of that road in return for land for adjacent parking may be part of the structure of a deal.

Additional circulation improvements in the longer term will require further cooperation and agreement among abutting landowners. The ability to improve safety on Noyac Road by minimizing curb cuts could be enhanced by combining Bay Avenue and Elm Street as shown in Sketch 6. Further study of this option is recommended as it involves a partnership effort between the Town and adjacent landowners.

Further study is warranted to enhance traffic safety in the Cromer’s Market area and along the entire Noyac corridor. There is strong community support and desire to accelerate funding of a study to identify and implement appropriate traffic/parking mitigation measures on a fast track basis.



Sketch 6: Illustrative Concept - Possible circulation and parking approach to Cromer’s Market area.

❑ **The Waterside Restaurant Site**

This site is located at the intersection of Noyac Road and Long Beach Road at the eastern edge of the project Study Area. The Waterside complex is situated on the north side of Noyac Road near the existing traffic circle. The subject property is 29,125 square feet (0.669 acre). The complex contains a leased restaurant/bar opening up onto a dockside water environment; the lower level currently functions as a private meeting space, made available by the owners at little or no cost to community groups and proportionately higher cost to individuals and organizations (see Figure 14). This lower level portion of the site is accessed through a separate property reportedly co-owned by the Waterside owner and the Town Trustees, adjacent to Town-owned parking/beachfront. Not only the building itself with its ‘seaside’ architectural feel, but even more the memories it represents from the events and informal gatherings hosted there, have been a long time part of Noyac’s collective consciousness.

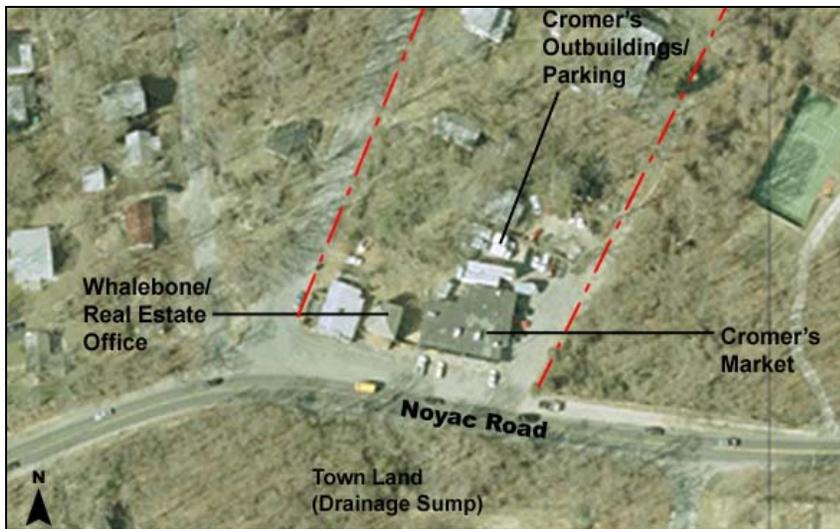


Figure 13: Existing conditions at Cromer’s Market area.

The concerns for this site differ from the previous two in that it is not as intensive in its commercial character as the previous two nodes. Further, the concerns expressed with regard to this site are related less to its current use and more toward its future re-use.

The building currently operates under a pre-existing, non-conforming exemption because it predates the neighborhood's residential zoning designation. Non-conforming uses can be changed only if the new use is found to have less impact on the neighborhood than the existing one. On December 18, 2003 the Southampton Town Zoning Board of Appeals (ZBA) made a determination that the proposed bay front residential condominiums would better conform to current zoning compared with the current use. The existing structure is proposed to be demolished and replaced with a two story building containing eight 1,200-square-foot units, including four one-bedroom and four two-bedroom apartments intended to attract younger couples or older 'empty nesters' rather than families with children.

Concerns raised by the neighbors have been largely based on a desire to keep the existing private function on the site--the restaurant/ bar is popular and the community space exists nowhere else in the hamlet--or to achieve purchase by the Town for use as a Community Center.

Although the Waterside site has received approval from the ZBA for redevelopment as condominiums, it still hinges on other Town and State approvals. Nevertheless, three potential options were considered during the planning process for this study and remain alternative uses in the event that the as presently conceived project cannot proceed.

- *Potential Option 1* - As proposed by owner, construction of approximately eight condominiums (see **Sketch 7**);
- *Potential Option 2* - As suggested by some community members, purchase by the Town and redevelopment as a community meeting space (see **Sketch 8**);

- *Potential Option 3* - A development similar to the proposed condominium development, but with a greater degree of community amenities (see **Sketch 9**).

The sketches on the following pages illustrate these three options.



Figure 14: View of The Waterside looking north on Noyac Road.

Proposed Development Approaches:

Common to each scheme is the reconstruction of the intersection of Noyac Road and Long Beach Road—utilizing the approach of an up-to-date roundabout designed to calm speeding traffic and provide an attractive westbound gateway (supplemented with attractive way-finding graphics) into the Noyac Hamlet Center. This design is part of the Town's pending streetscape application to New York State.

Similarly, each scheme also includes approaches to slow traffic; minimize curb cuts for new development adjacent to the Noyac Road hill, west and south of the traffic signal; improve pedestrian safety through the provision of pedestrian cross-walks; and build bicycle lanes through the proposed traffic circle extending a bike lane project on Route 114 in North Haven.