

- consolidation or sharing of utility rights by common agreement for purposes of septic calculations and allowing transfer of Pine Barrens or other development rights if a density increase is agreed by the Town;
- in addition, as per 1999 Comprehensive Plan Update recommendations, office uses in the MUPDD should allow medical services (not allowed in HO zone)
 - opportunities for additional open space as an identified public benefit, established as a Planned Development District requirement;
- more detailed definition of allowable sizes, hours or other controls for associated or accessory uses;
- creation of shared access ways for autos and pedestrians, connecting adjacent sites;
- abandonment of unneeded rights-of-way such as the northern section of Nowedonah Avenue, in order to unite adjacent open space parcels;
- sharing of parking for adjacent developments such as retail or office uses and restaurant uses.

The intent of the MUPDD is to facilitate desired residential or other uses not otherwise allowed while building in the opportunity to increase useable open space in the hamlet center. The participation of the Community Club is not required (if septic and development rights are purchased and transferred from other locations) although a partnership solution represents an opportunity for both the Community

Club and adjacent landowners to achieve their own objectives in the context of hamlet center improvement.

4.2 Design Guidelines and Beautification

This plan also proposes a recommended design approach for hamlet center development. This takes the form of two categories:

- building design and siting guidelines specific to the Water Mill hamlet center, such as would be used for review of future projects by the proposed Design Review Board; and
- beautification proposals, which can be the basis for public or public-private actions in implementing hamlet-wide improvements, including specific project proposals made possible through the proposed Mixed-Use Planned Development District (MUPDD) .

Attention to both of these categories is critical to the maintenance of Water Mill Center's unique visual and historic ambiance.

1) Siting and Building Guidelines

New town-wide design principles, guidelines and procedures for use by the proposed Design Review Board (advising the Town Planning Board) have been prepared and are currently under consideration for approval.

These considerations for review of projects include a specific section for hamlet center areas, examining recommended site design considerations, relating proposed buildings to their surrounding context, as well as building design considerations. The process also makes provision for additional hamlet-specific guidelines as needed, which are intended to be created through studies such as this Strategy.

The Water Mill-specific considerations described on the following pages will therefore be an additional level of detail for Design Review Board advice to Planning Board in consideration of future projects in this district:

A Site Design

Relate building location and landscaping to surrounding natural and man-made context. *This should include particular emphasis on maintenance of the historical character and environmental ambiance of the Water Mill Hamlet Center.*

A1 Relationship to Natural Context:

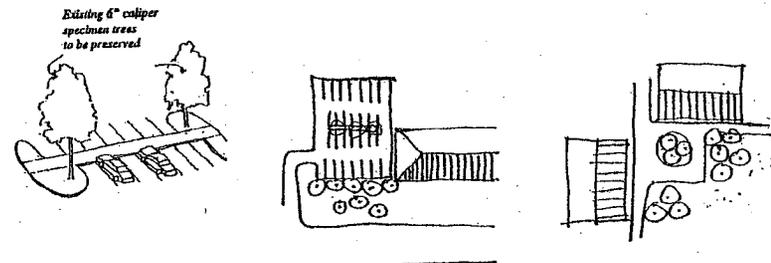
Buildings should preserve and enhance characteristic environmental or natural features. *Specimen trees along Nowedonah Avenue and on the Water Mill Village site should be preserved wherever possible as part of proposed open space or within parking areas. Similarly, trees in the area west of Station Road should be preserved where possible to provide amenity for the suggested 'Water Mill Mews' development. New trees should be added along new roadways and walkways.*

A2 Relationship to Built Context:

Building siting should create a positive relationship with surrounding structures and the adjacent roadway. *Residential development, inn or assisted living buildings should be set back an appropriate distance from the Montauk Highway roadway. New infill buildings in the 'Water Mill Mews' area should shape attractive public spaces. Setbacks should be similar to surrounding context (with use exceptions as necessary).*

A3 Circulation/Parking:

Pedestrians and landscaping, not cars and asphalt, should be the primary visual focus. *Parking for new uses on 'Water Mill Village' site should take place adjacent to and not in front of new building. Parking for the 'Water Mill Mews' area between Station Road and Water Mill Square should be located to the north, adjacent to the rail tracks and along new roadways. Pedestrian walkways should be added along Nowedonah Avenue, Station Road, and Montauk Highway and along new roadway connections (by cross access agreement). On-street parking, which creates a safety barrier and calms passing traffic, should be provided as appropriate where space and traffic flow permit.*



A4 Landscaping:

Appropriate plant materials, walls, pedestrian elements or earthwork should be used to provide an attractive walkable environment and mitigate negative visual impacts. *Residential buildings should be buffered from highway by landscaped berms. Roadways and parking areas*

should be articulated by street trees. Interconnecting walkways should be attractively landscaped.

B. Building Design

Maintain the look and feel of surrounding structures and community character.

B1 Building form and scale:

Buildings should relate to their community context in terms of similar or compatible massing, height and roof treatment.

In the historic Water Mill center, lot sizes vary as do building sizes. This look should be encouraged and merger of lots to create larger lots if this leads to larger buildings should be discouraged. New structures should maintain the residential scale of Water Mill, with sloped roofs and maximum height two stories (three-story by MUPDD special exception review for assisted living residential). New infill buildings in 'Water Mill Mews' area should reflect house/ barn motifs.

B2 Building facades:

Building façades should provide human-scaled design—articulation of surface elements, exterior shelter and semi-public space.

Entries to new buildings should be defined with porch or portico elements. Residential buildings where possible should frame outdoor plazas. Mews buildings should also relate to informal seating areas and walkways.

B3 Building appearance:

Buildings should relate to their community context in terms of appearance from the right of way, reinforcing over time the character specific to the local area.

Buildings should be consistent with existing domestic and historic character of hamlet center—use of wood and/ or brick, with predominantly light-colored trim or siding.



B4 Signage

Building signage should follow the new guidelines established for the Town as a whole, covering signage placement, size, material and design.

In addition, in order to maintain the character of the existing area, specific criteria for the Water Mill hamlet center should include:

- *Prohibition of interior and exterior neon signage*
- *restriction on height of freestanding signs to 4'-0*
- *emphasis on wood signs, with shielded external lighting, replacing older signs as possible.*
- *As part of its ongoing signage analysis, the Town should investigate the potential of amortization as a way to phase out non-conforming signage.*

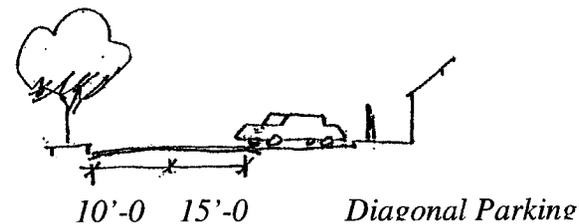
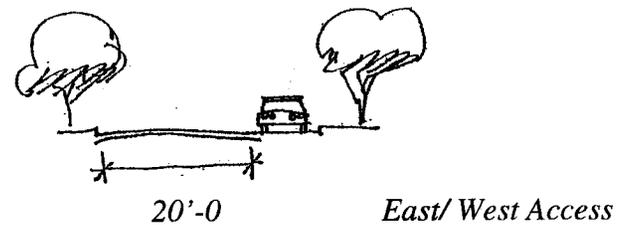
2) *Beautification*

The illustrative plan (on page 38) illustrates the proposed beautification approach for the Water Mill hamlet center. It is based on the following local standards for public sector projects, community- sponsored improvements, or private development implemented as part of this larger plan:

Internal streets:

- the proposed east-west internal connection, created through shared-access agreements among adjacent landowners, should be built to similar specifications as a narrow public street: two moving lanes of 10'-0" width (one in either direction) with occasional parallel parking (15'-0" width lanes if adjacent to diagonal parking).
- new access streets perpendicular to Montauk Highway should be one-way streets with two moving lanes of 10'-0" width in same direction (if diagonal parking is feasible, lane width adjacent to diagonal parking should be 15'-0").
- curb and gutter, with concentrated but limited access points to adjacent parking.

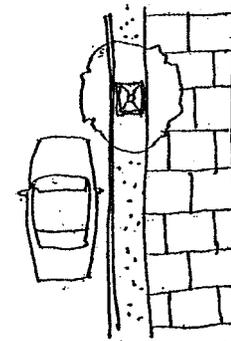
- in order to limit speed and pedestrian conflicts, streets should be punctuated at major intersections by stop signs flanked by well-articulated crosswalks
- because parking is often provided in adjacent existing lots, new access streets perpendicular to Montauk Highway will not have on-street parking (with the exception of parking proposed along the east side of Station Road, made possible by a switch to one-way traffic, proposed as a corrective response to an existing accessibility problem for adjacent merchants).



• *External Street (Montauk Highway):*

- Montauk Highway should continue its policy of allowing short-term parallel parking in selected locations near retail establishments (there is no room adjacent to Montauk Highway for diagonal parking, which would require backing onto a busy highway rather than pulling in facing forward).
- Bicycle traffic should be detoured off Montauk Highway to a route along the access way parallel and adjacent to the rail line.
- Sidewalks along Montauk Highway should be extended so as to form a continuous pedestrian way along the ‘frontage’ of the hamlet center.
- Paving details should continue existing edging and other motifs, but concrete sidewalk areas can be upgraded over time with simple colored aggregate concrete articulated by staggered joints. Although brick walks extend to the curb in front of retail uses, landscaped verges should be added between sidewalk and curb in areas adjacent to office or residential use.
- Lighting poles for pedestrian lighting (low and in scale with adjacent buildings) should provide a consistent even spread of light, punctuated by street lights at major intersections provided as required for

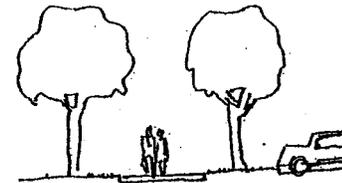
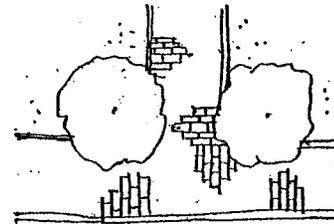
safety. Other dusk to dawn flood lights should be prohibited, except for safety considerations. Light should project downward with no uplift, minimizing spillage and glare.



Sidewalk detail

Internal Walkways:

- Walkways should have a distinctive pedestrian feel. Suggested are brick walks with paver edging.
- Trees and adjacent grass areas should line the edge of pedestrian corridors. Small plaza areas should be created at intervals, featuring widened pavement, benches, and art.
- Where walkways cross new access roads or parking areas, landscaped ‘bumpouts’ should be used to minimize width of pavement and maximize related locations for trees or other landscaping at these important pedestrian nodes. Crosswalks should be 6’-0 wide walkways, articulated with blue-gray concrete pavers and handicapped-accessible sloped curbs.
- Explore establishment of a pool of funding from grants, revolving loans, and/ or improvement district assessment dedicated to assisting merchants/ landowners with cost and implementation, tied to facade renovation or other efforts.



Internal Walkways