



TOWN OF SOUTHAMPTON
Pattern Book for Hampton Bays, New York

Data Gathering Phase Findings / 1.27.2017

HISTORICAL CONCEPTS
ARCHITECTURE & PLANNING



The Town of Southampton (“The Town”), working with the Hampton Bays Civic Association, the Hampton Bays Economic Development Task Force, and individuals within the community has defined a study area within the Hamlet of Hampton Bays designated as the Hampton Bays Downtown Overlay District (“Overlay District”). They determined that redevelopment strategies were needed in order to encourage sustainable downtown revitalization and economic development activities in this area.

Recognizing that the Overlay District currently lacks a cohesive expression of the local and historical community patterns, and the architectural styles and elements preferred by the citizens, The Town has commissioned Historical Concepts to author a Pattern Book that will serve as the basis for a future Zoning Overlay. This Pattern Book is intended to define and describe the desired forms of future development and identify the way in which individual buildings within the Overlay District should relate to streets and public open spaces.

The goal of the Pattern Book is to synthesize the past planning initiatives and studies, the local and historical patterns of development, and local architectural expressions in order to provide a blueprint for future development. As a first step in the process of developing this Pattern Book, Historical Concepts has embarked on a Data Gathering Phase to examine local patterns and document existing conditions in Hampton Bays. This document represents a summary of the findings from that Data Gathering Phase and is presented as the basis for the future Pattern Book for the Hampton Bays Downtown Overlay District.



CONTENTS

1. LOCAL PRECEDENT 7

Amagansett, NY 10

Babylon, NY 12

Bay Shore, NY 14

Bellport, NY 16

Bridgehampton, NY 18

East Hampton, NY 20

Greenport, NY 22

Huntington, NY 24

Northport, NY 26

Patchogue, NY 28

Port Jefferson, NY 30

Riverhead, NY 32

Sag Harbor, NY 34

Sayville, NY 36

Southampton, NY 38

Southold, NY 40

Westhampton Beach, NY 42

2. HAMPTON BAYS 45

3. FIGURE GROUND STUDIES 59

4. INITIAL STAKEHOLDER & PUBLIC INPUT 63

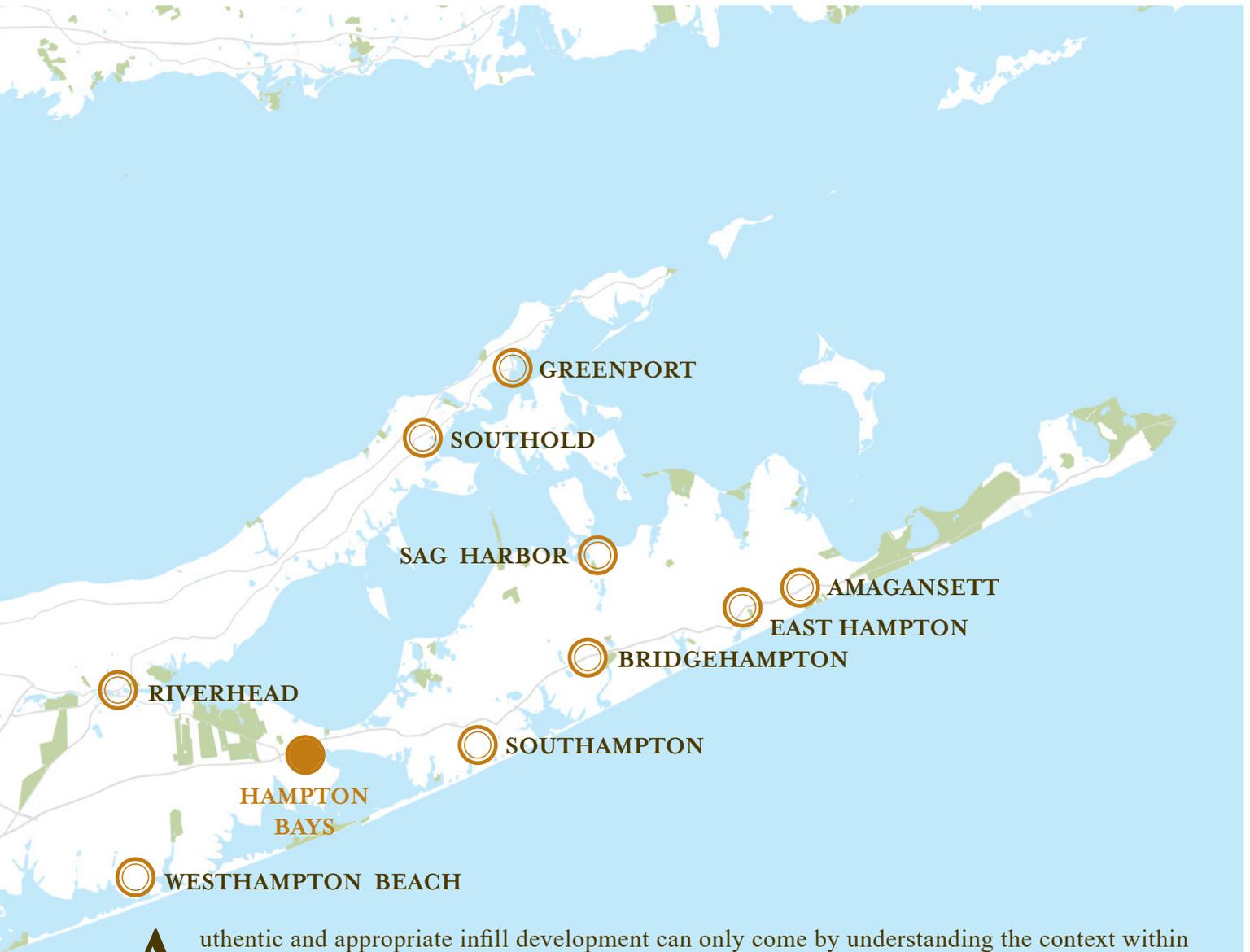
5. REVIEW OF PAST PLANNING INITIATIVES &
 OBJECTIVES FOR THE PATTERN BOOK 79



1. Local Precedent



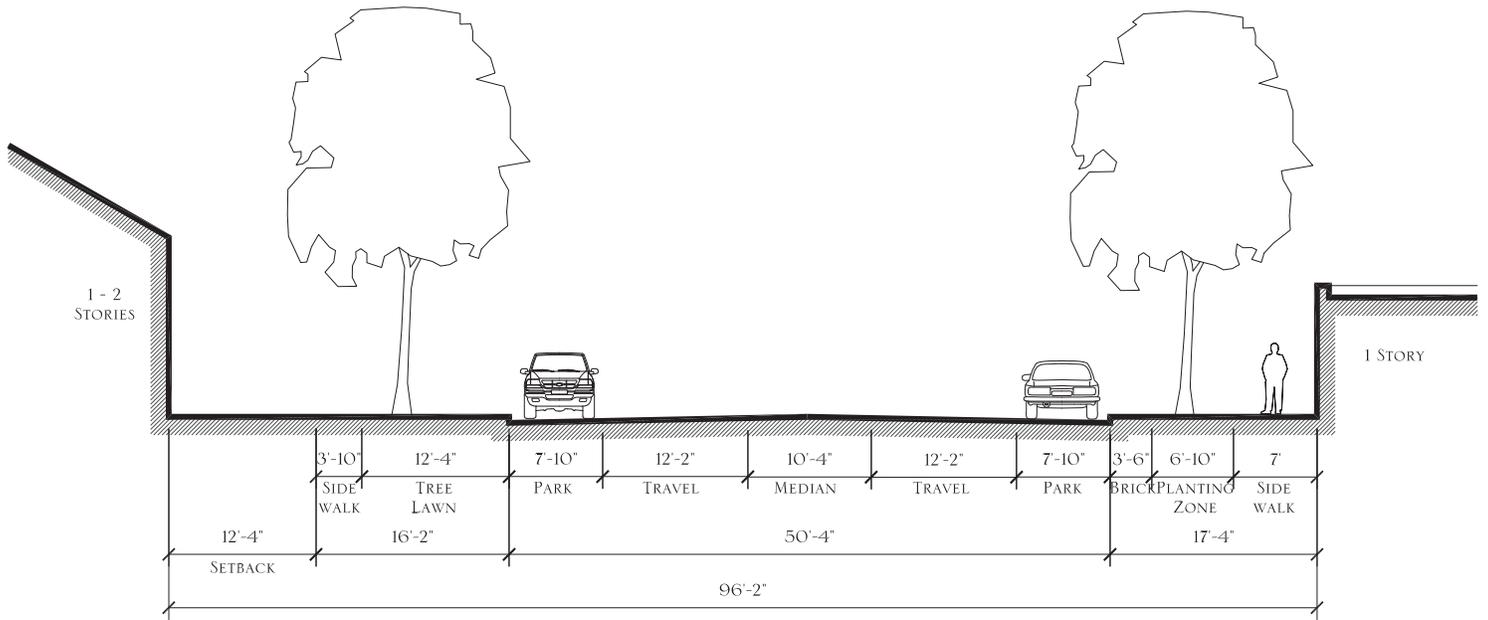
Long Island Precedent



Authentic and appropriate infill development can only come by understanding the context within which one is working. Historical Concepts' approach to design, whether a small farm building or new town, always begins with local precedent. In order to prepare for the creation of the **Hampton Bays Downtown Overlay District Pattern Book**, we began by studying the DNA of Long Island hamlets and villages. We focused primarily on the main street corridors of these communities to guide our analysis of downtown Hampton Bays' main street (Montauk Highway). Hampton Bays should not be a replica of any other hamlet, but we fully believe that there are valuable lessons to be learned from the successes and failures of places within about an hour's drive. The map on this page identifies the places provided by recommendation of the **Town of Southampton** and observed by **Historical Concepts**. These places all represent maritime hamlets that grew in a similar era – some grew more than others. Together they represent a spectrum of scales, densities and characters which should inform any new development for Hampton Bays.

We refer to our visits to these places as a **Precedent Tour**, a reconnaissance effort whereby in short order we aim to quickly identify and document the DNA of the place. We do this by walking the sidewalks, sitting in the parks, talking with locals, measuring street widths and building heights, analyzing maps, and taking countless photographs. In doing so, we not only immerse ourselves in what makes a place and region unique, but we also arm ourselves with an incredible amount of data. The following pages represent a comparative selection of data gathered from each hamlet or village.

AMAGANSETT, NY



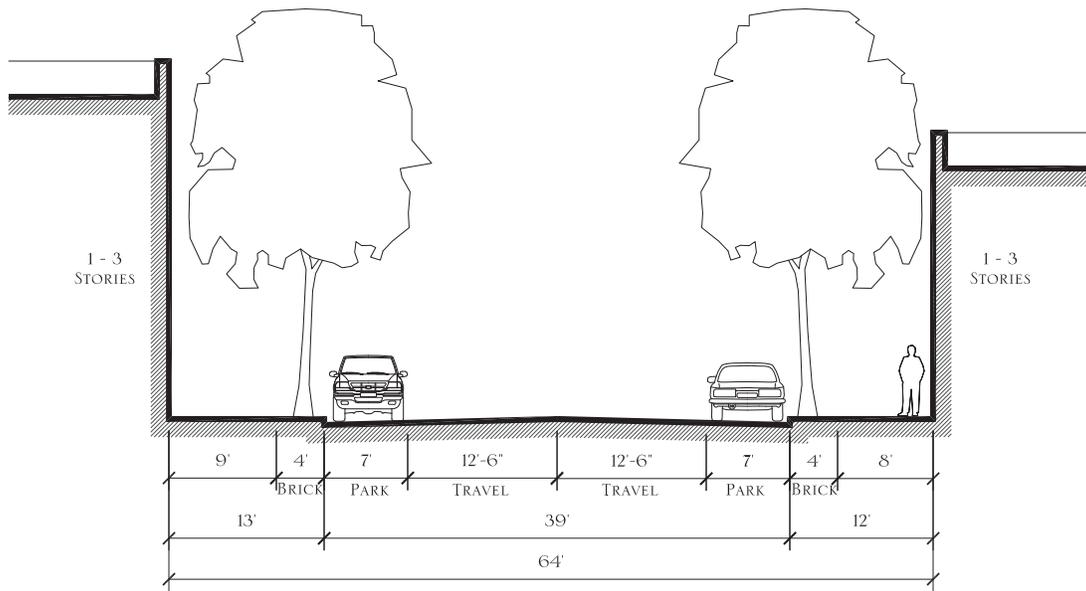
Typical Street Section "A-B"



IDENTIFYING FEATURES

1. Hamlet; Consistent street frontage for two blocks along Main Street
2. On-street parallel parking on both sides, one driving lane each way, large turning lane/no passing median
3. Wide sidewalks with dining, walkway, trees within a planting strip, an occasional bench, and flowers
4. Parking is located on street with an additional midblock large parking lot in the rear of buildings located along Main Street (Route 27)
5. Green square fronts on Main Street (Route 27), lined with retail uses in residential-scale buildings with a lawn setback
6. Most buildings are detached, one story with traditional cornices, pitched roofs; a mix of brick and wood buildings





Typical Street Section "A-B"

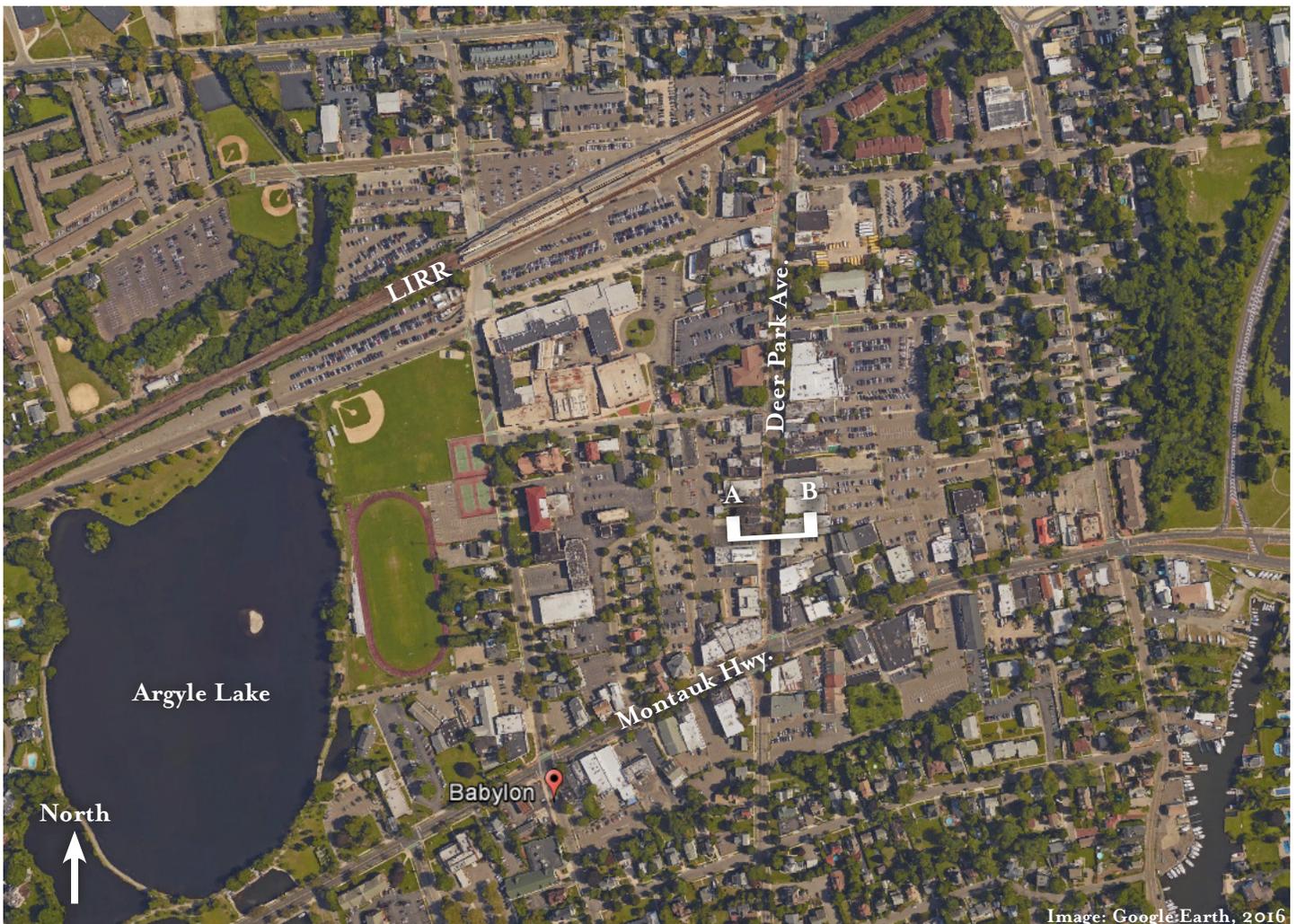
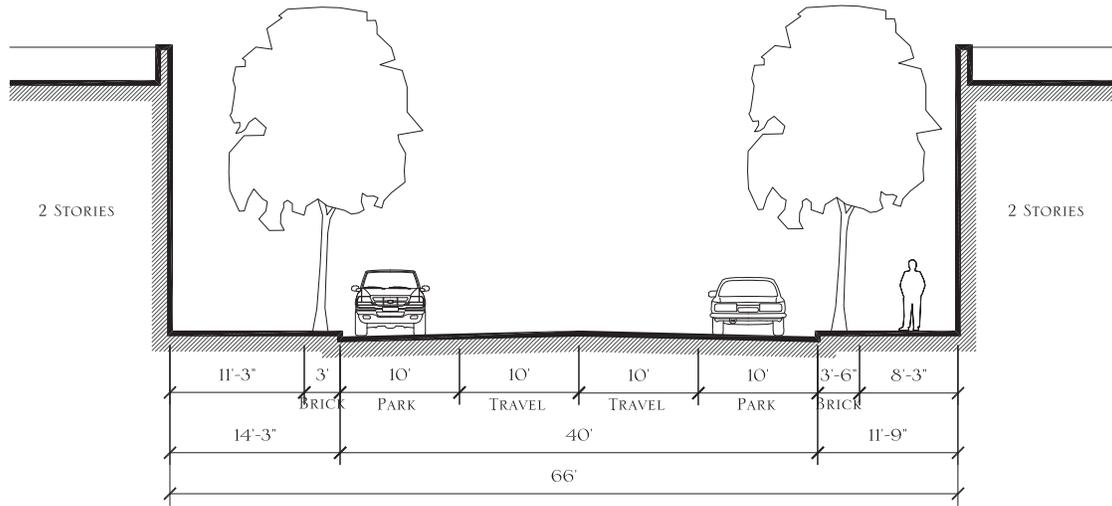


Image: Google Earth, 2016

IDENTIFYING FEATURES

1. Village; Consistent street frontage for several blocks along two major intersecting streets
2. Onstreet parallel parking on both sides, one driving lane each way along Deer Park Ave; Montauk Highway widens at intersection for a left turning lane
3. 8'-0" sidewalks with additional 4'-0" buffer; widens in a few locations where newer or civic buildings are set back
4. Ample parking is located behind most buildings, accessible mid-block; train station is within walking distance from "downtown"
5. No formal greenspace along Montauk Highway aside from treescape
6. Majority of buildings are two to three stories, attached; brick, with parapets and flat roofs and appear to be historic with some alterations





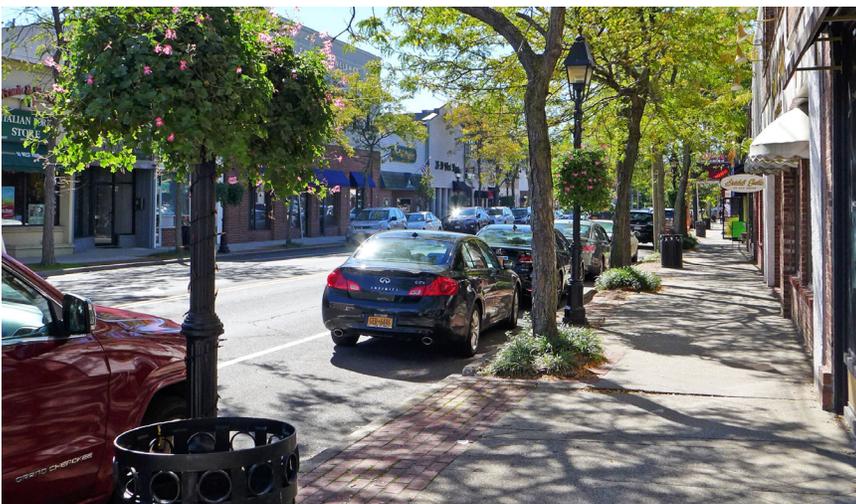
Typical Street Section "A-B"

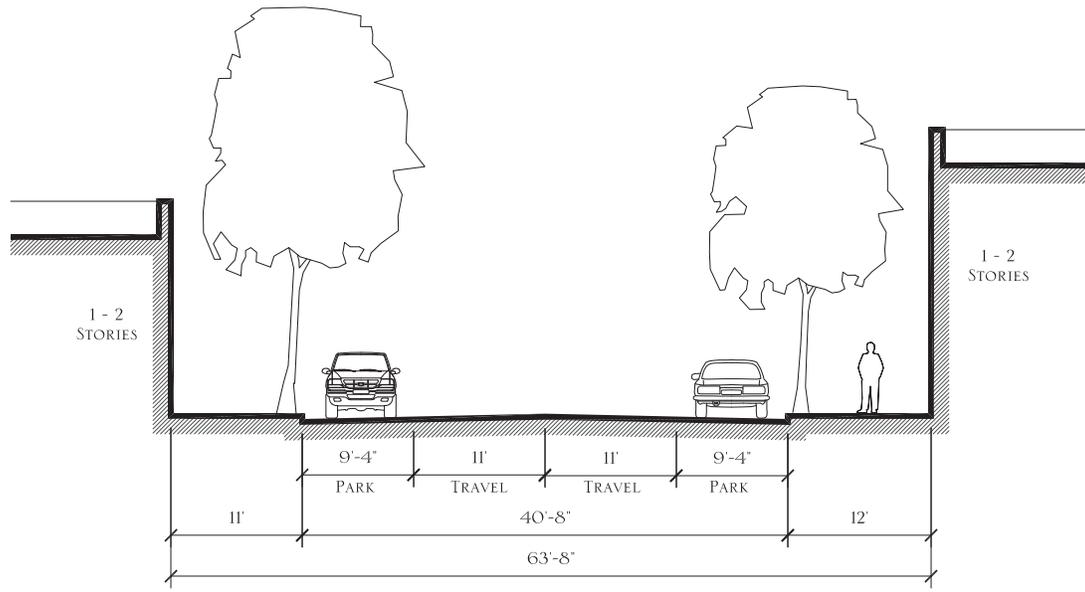


Image: Google Earth, 2016

IDENTIFYING FEATURES

1. Hamlet; Consistent street frontage continues for about four blocks along Montauk Highway
2. Onstreet parallel parking on both sides, one driving lane each way
3. Sidewalk widths vary from 8'-3" to 11'-3" with a 3'-0" to 3'-6" brick buffer, trees, and street lamps
4. Ample parking is located behind each building, connected drive lanes for access.
5. Greenspace is located towards the water off Montauk Highway, with several landscaped passthroughs and repurposed infill sites
6. Many historical brick buildings with parapets and flat roofs; two to three stories tall; several refined civic and institutional buildings along the street





Typical Street Section "A-B"

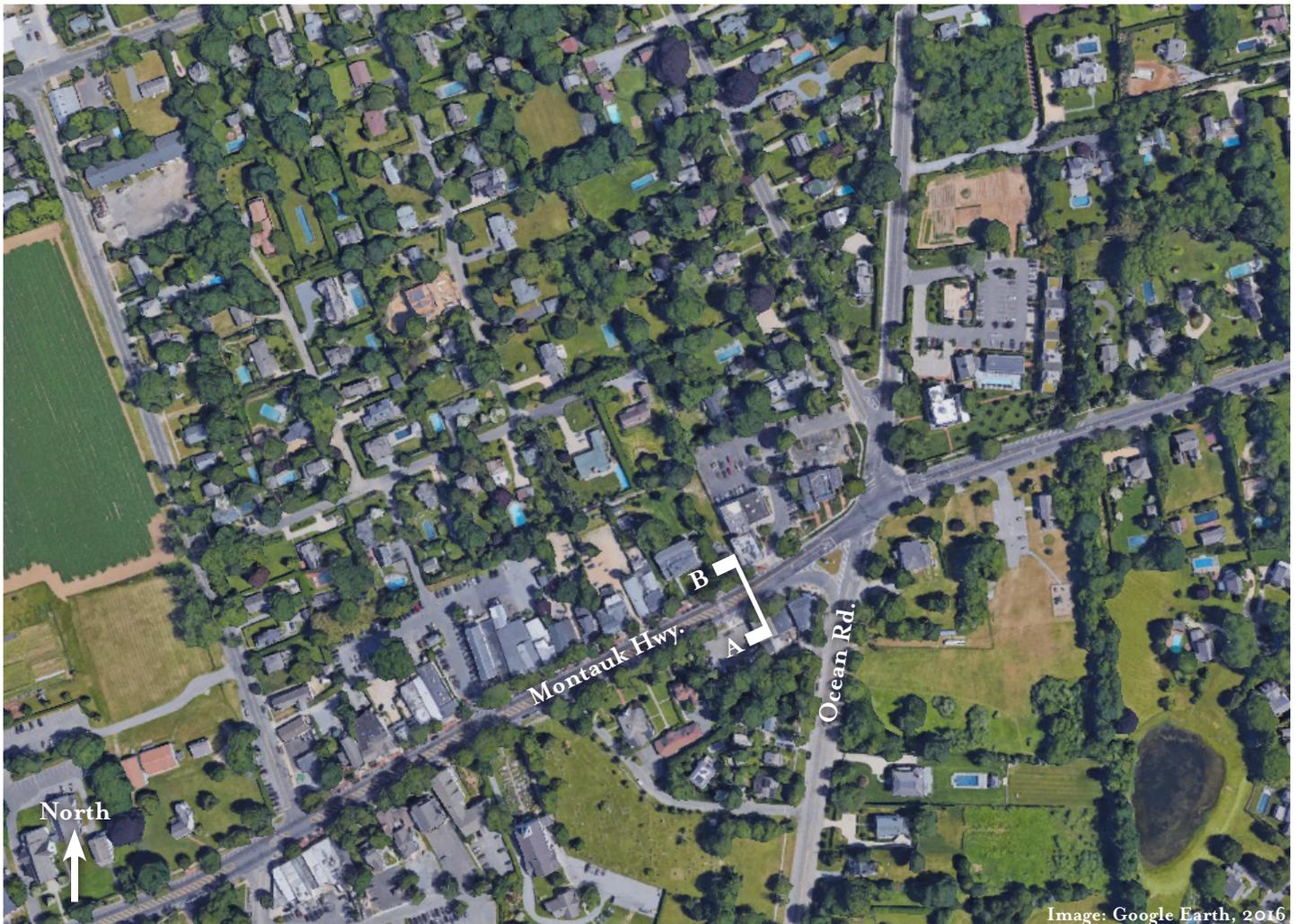
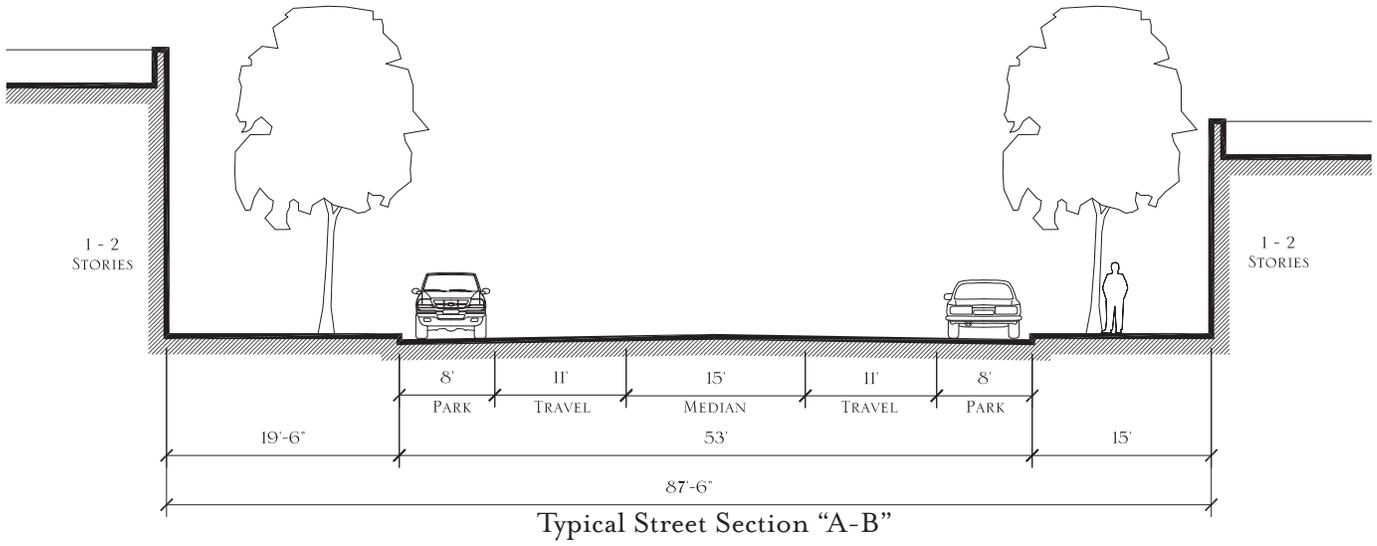


Image: Google Earth, 2016

IDENTIFYING FEATURES

1. Village; Consistent street frontage for one block along Country Road; additional block of suburban-style retail to the south
2. Onstreet parallel parking on both sides, one driving lane each way
3. Sidewalk widths vary from 8'-0" to 9'-0" with 3'-0" brick buffer; seating for restaurant, benches and streetlights
4. Parking is located on street with additional parking behind buildings and in municipal lot one block away
5. No formal greenspace aside from treescape along the main street (S Country Rd); small park one block away
6. Most buildings are one to two stories, detached; there are some pitched roofs with simple cornices and many newer buildings and additions from the past half century; distinct variety of character and style



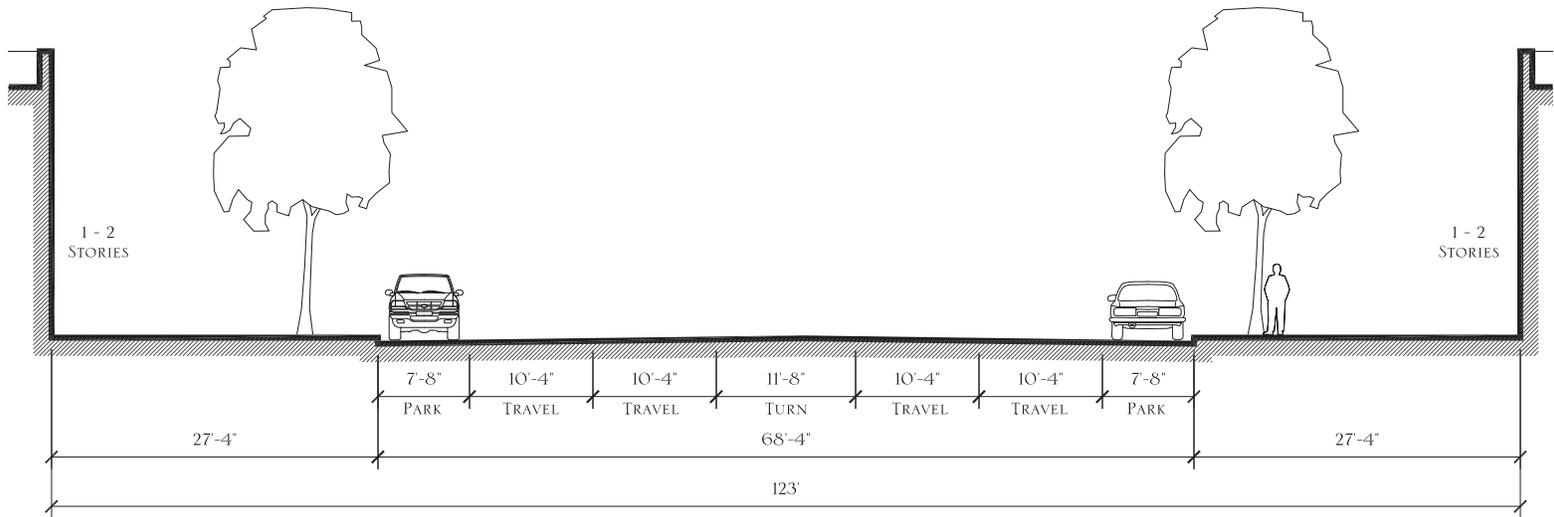


IDENTIFYING FEATURES

1. Hamlet; Consistent street frontage for one long block, primarily only along the north side of the street
2. Onstreet parking, one driving lane each direction
3. 19'-6" sidewalk with large brick median with trees and benches
4. Onstreet parking; additional parking is located mid-block behind several stores on each side
5. Much of the south side of Montauk Highway is greenspace with several churches and a cemetery
6. One and two story buildings, attached; Pitched roofs, variety of masonry and wood construction with some stucco, traditional details



EAST HAMPTON, NY



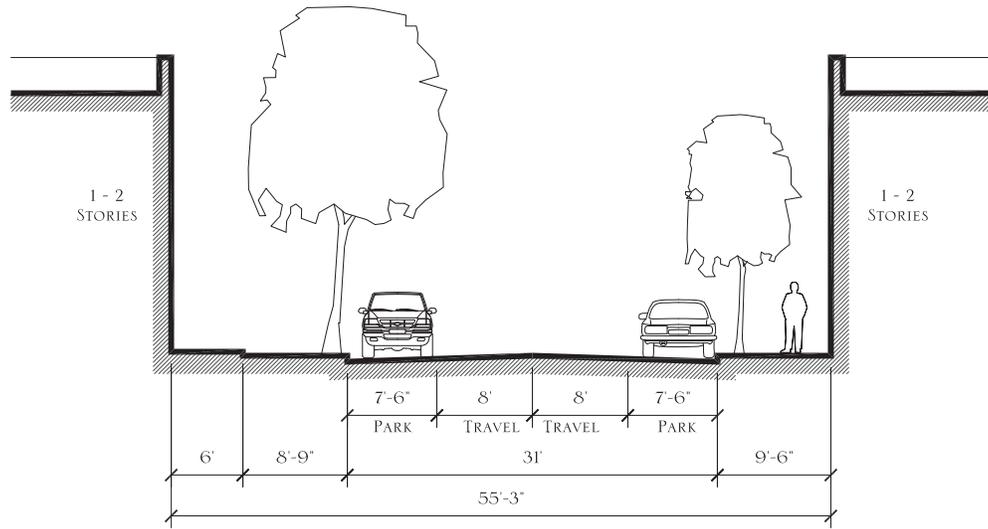
Typical Street Section "A-B"



IDENTIFYING FEATURES

1. Village; Consistent street frontage for several blocks along two major intersecting streets
2. On-street parallel parking, two lanes with additional turning lanes along the main street (Montauk Highway); onstreet parking and two lanes eastbound, no passing median, and one lane westbound along Newtown Lane
3. Large sidewalk width including a large brick planting area for trees; benches and street lamps along the brick median
4. Parking is located on street with additional mid-block parking behind Montauk Highway and Newtown Lane.
5. No large greenspace this area of Montauk Highway; large green space located several blocks to the south
6. Predominantly one and two story buildings, attached; Variety of building character and style; wood and brick construction; typically pitched roofs along Newtown Lane and parapets/flat roofs along Main Street





Typical Street Section "A-B"

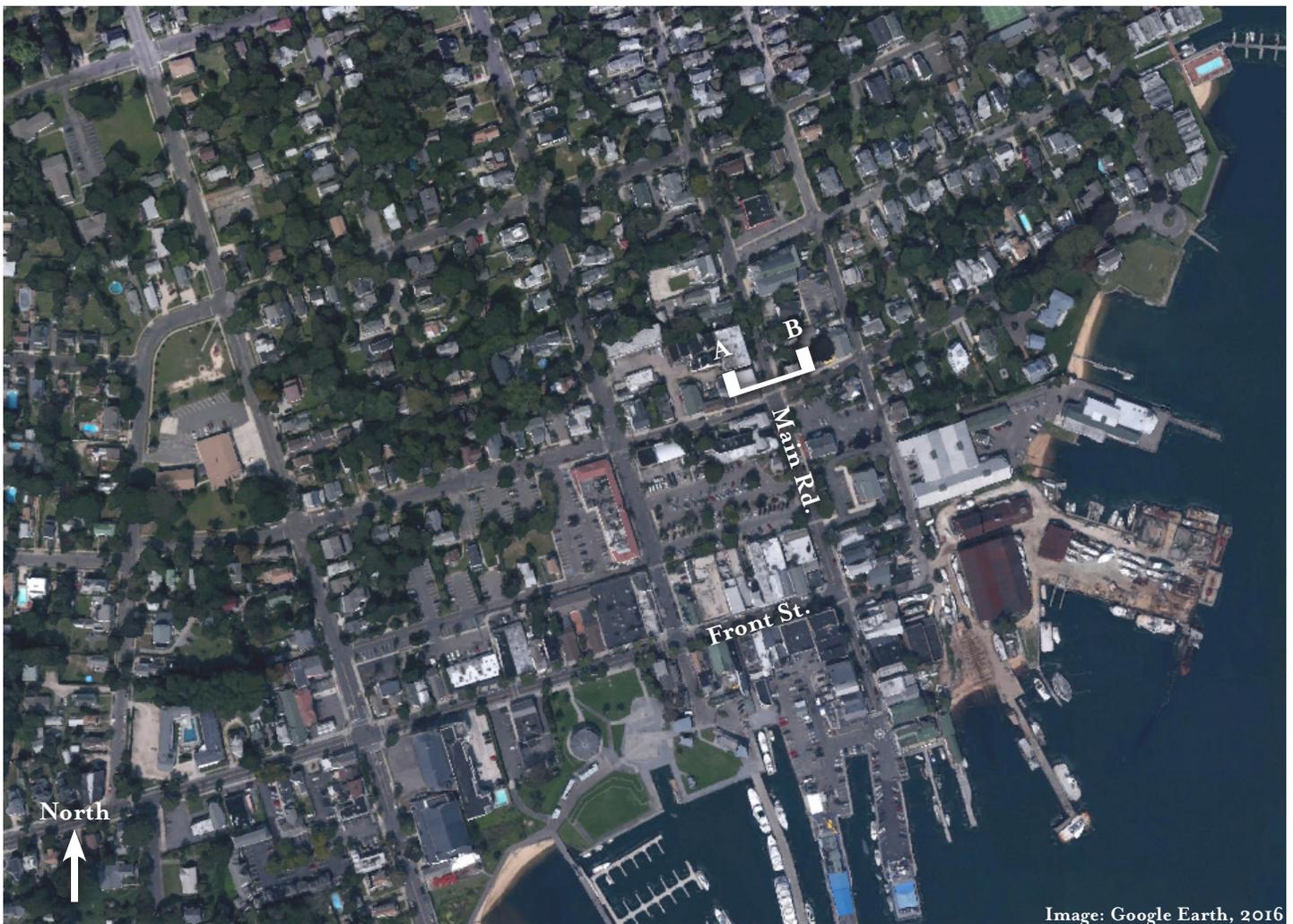
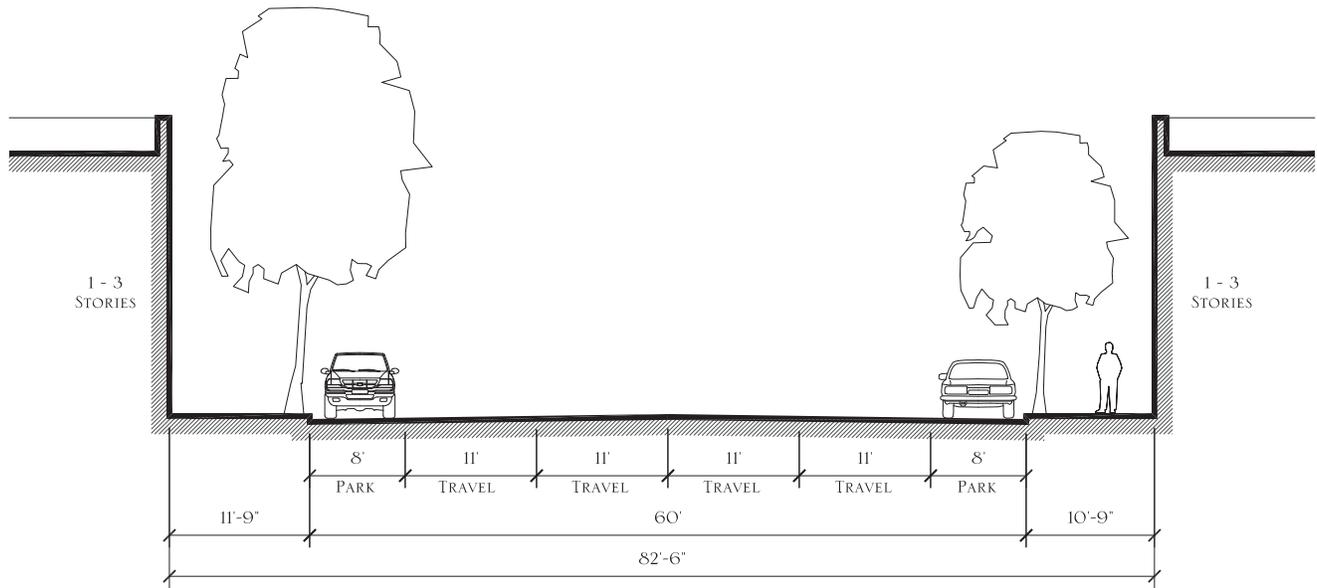


Image: Google Earth, 2016

IDENTIFYING FEATURES

1. Village; Consistent street frontage along two major intersecting streets for about three blocks
2. Onstreet parallel parking on both sides of Main Road; North side only along Front Street; one driving lane each direction;
3. Varying sidewalk widths with occasional deep raised steps, all with buffers, trees, and cobra street lights
4. Parking is located on street with additional mid-block parking behind Main Road and several parking lots along Main Road; nearby train station and ferry terminal with parking lot south of Front Street
5. Large park along Front Street, fronting the water
6. Predominantly one and two story buildings, attached; Three-story buildings along one block of Front Street; Variety of building character and style; wood and brick construction; typically pitched roofs along Main Road and parapets with flat roofs along Front Street



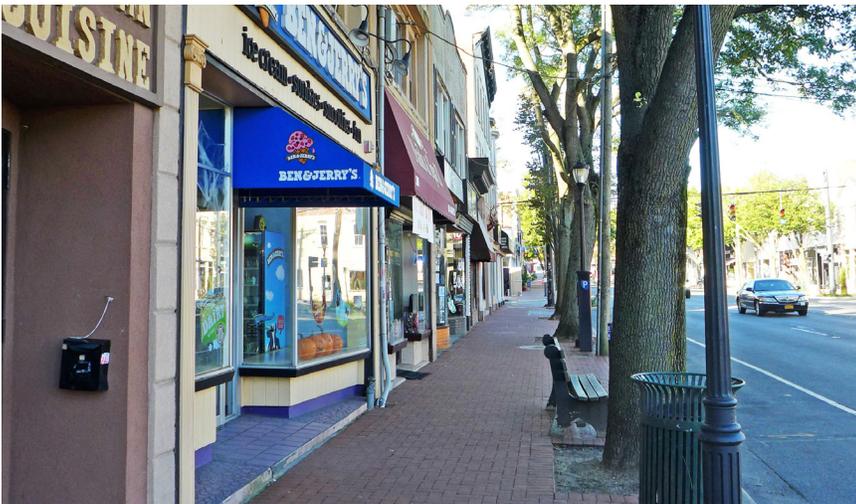


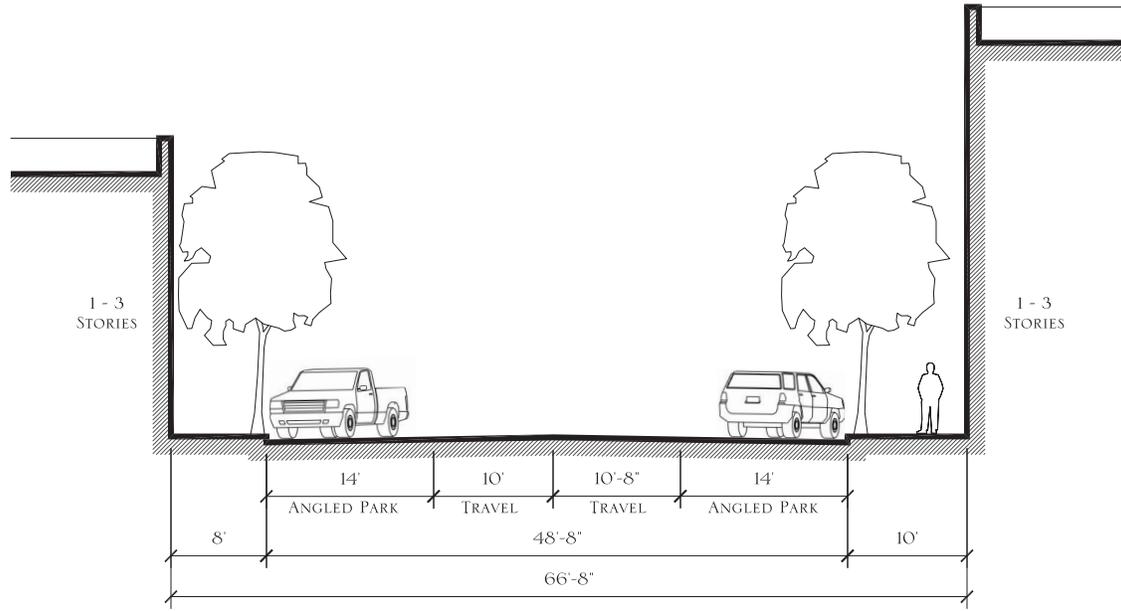
Typical Street Section "A-B"



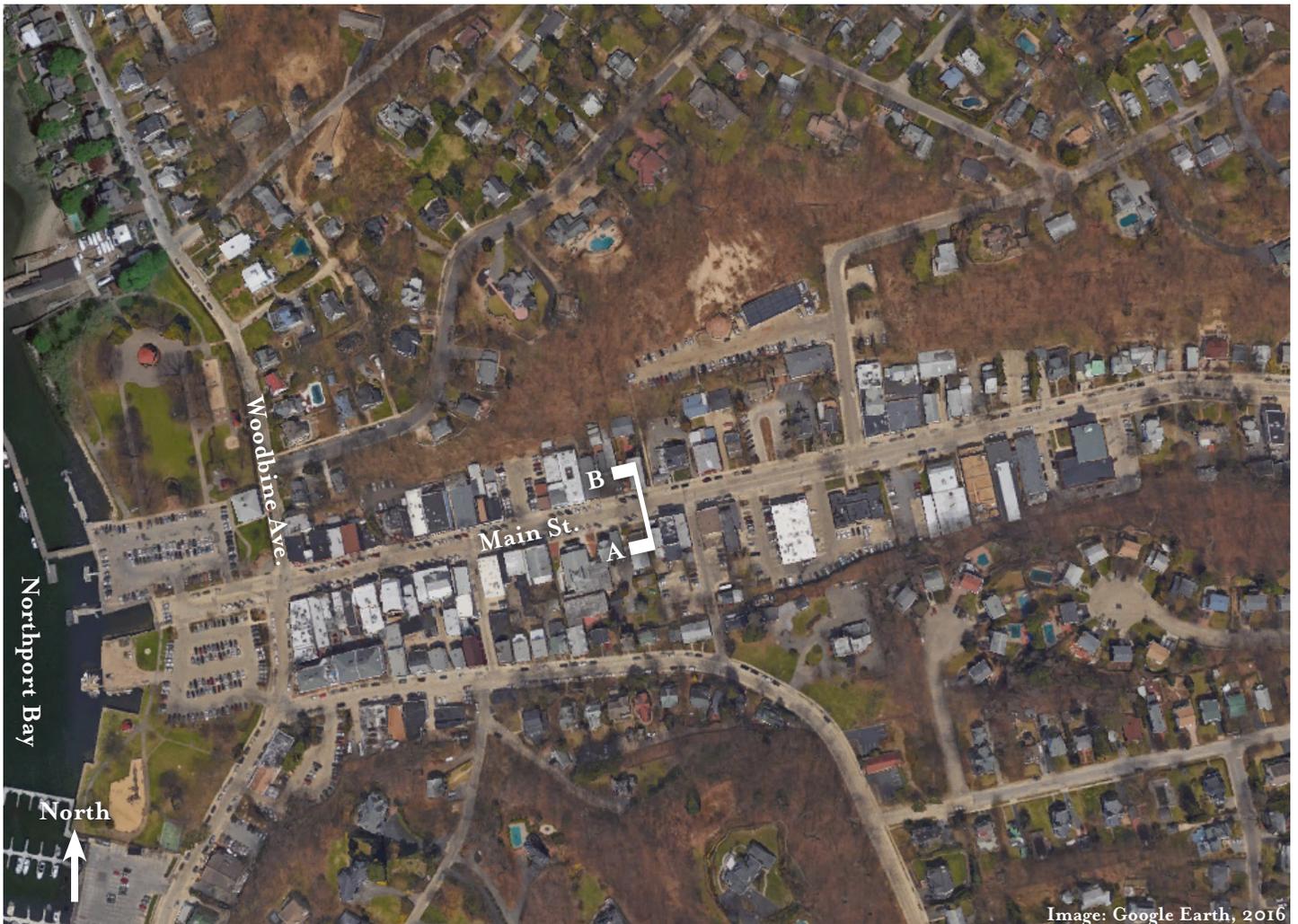
IDENTIFYING FEATURES

1. Town; Consistent street frontage for four blocks along Main Street and continuing along two cross streets; closest town to New York City in our study
2. Two driving lanes in each direction
3. 10'-4" to 11'-9" brick sidewalks, with buffer zone of trees, street lamps, and benches
4. Parallel parking along Main Street and New York Avenue with midblock parking lots fronting side streets
5. No greenspace in core downtown; cemetery at eastern end of Main Street
6. Predominantly two and three story buildings, attached; Most buildings are brick or stone masonry; parapets with flat roofs





Typical Street Section "A-B"

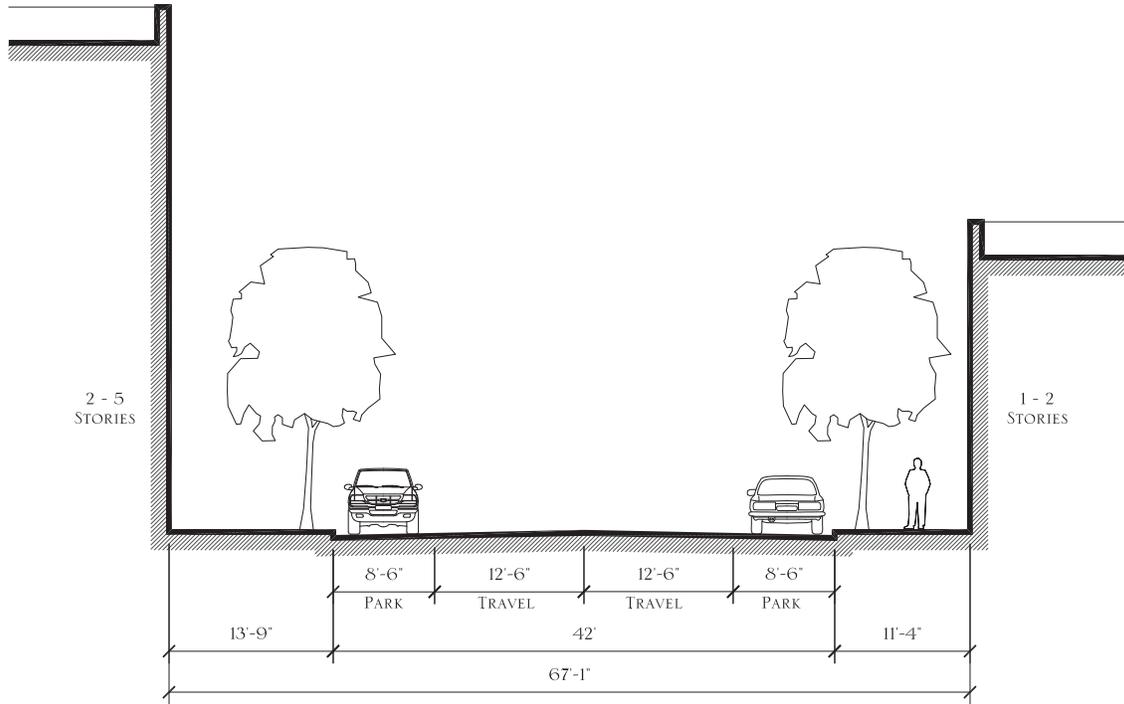


IDENTIFYING FEATURES

1. Village; Consistent street frontage for several blocks along Main Street, and continuing for a block on Woodbine Ave; Main Street terminates into the harbor; large amount of topographical rise to either side of Main Street
2. Angled on-street parking in both directions with large driving lanes, one in each direction
3. 8'-0" to 10'-0" sidewalk widths with trees, benches, and street lamps
4. Onstreet parking on both sides with several municipal lots mid-block and along waterfront
5. Large Public park along waterfront
6. Predominantly three story buildings closer to the harbor with a mix on one and two stories further east, attached; Many brick and stone buildings with elaborate cornices



PATCHOGUE, NY



Typical Street Section "A-B"

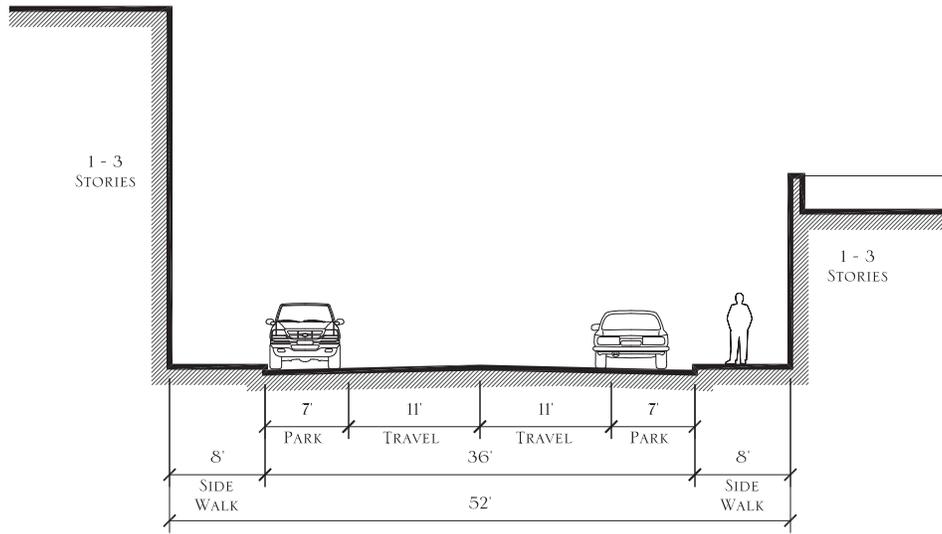


Image: Google Earth, 2016

IDENTIFYING FEATURES

1. Village; Consistent street frontage for two blocks along Montauk Highway; Newer mixed-use development at western end of the main street (Montauk Highway), facing a mid-block plaza and fronting the street one block to the North of Montauk Highway
2. On-street parallel parking on both sides, one driving lane each direction
3. Sidewalk width varies from 11'-4" to 13'-9" including consistent 3'-0" buffer of trees, benches, and street lamps
4. Parking is onstreet and additionally located midblock behind buildings along Montauk Highway
5. There is no park-like greenspace along Montauk Highway aside from treescape; several small parks are nearby
6. Predominantly two and three story buildings along Montauk Highway, with some three story buildings, attached; Newer development is four stories; Most buildings along Montauk Highway are masonry or wood; several civic buildings including a bank, library, and post office are detached from typically attached fabric buildings





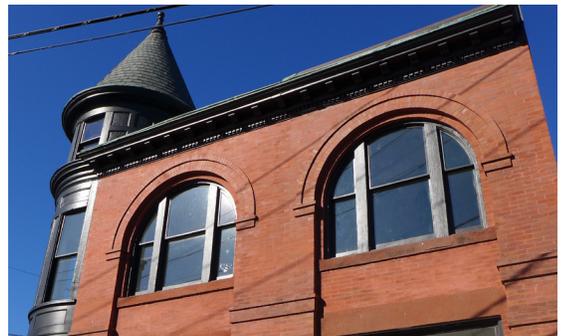
Typical Street Section "A-B"

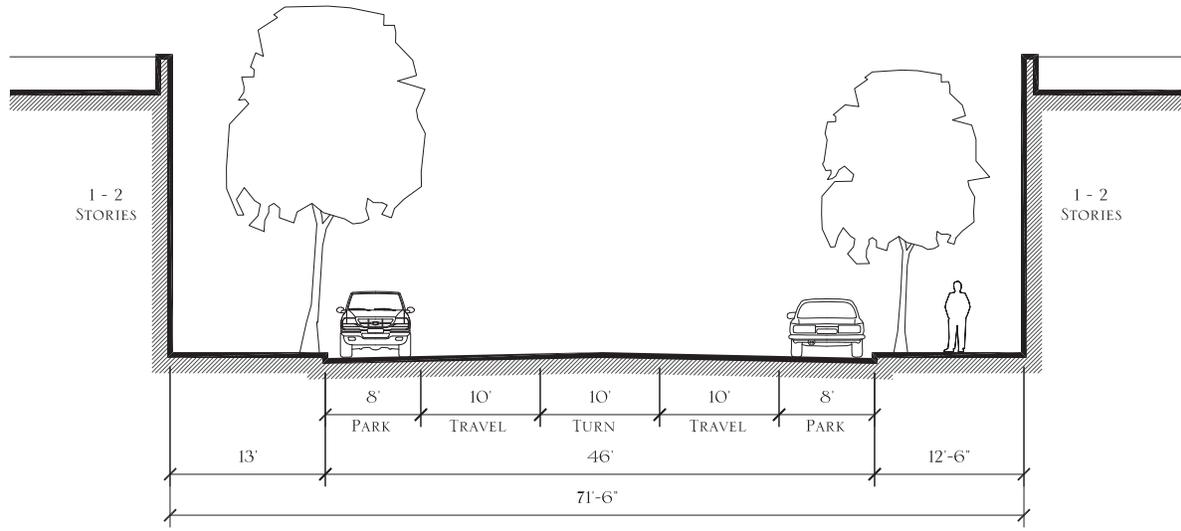


Image: Google Earth, 2016

IDENTIFYING FEATURES

1. Village; Consistent street frontage for several blocks along Main Street, continuing on two cross streets and a pedestrian mall; Main Street terminates into harbor
2. One driving lane in each direction with parallel parking on both sides of street
3. Narrow sidewalks are interspersed with trees, lamps, and telephone poles
4. On-street parallel parking on both sides with several municipal lots mid-block and along waterfront
5. Several small sports fields are located southwest of Main Street
6. One to three story buildings, attached; Most buildings have wood cladding; predominantly parapets with occasional Greek or Victorian detailing and flourishes at cornices and entrances; civic buildings are almost exclusively brick





Typical Street Section "A-B"

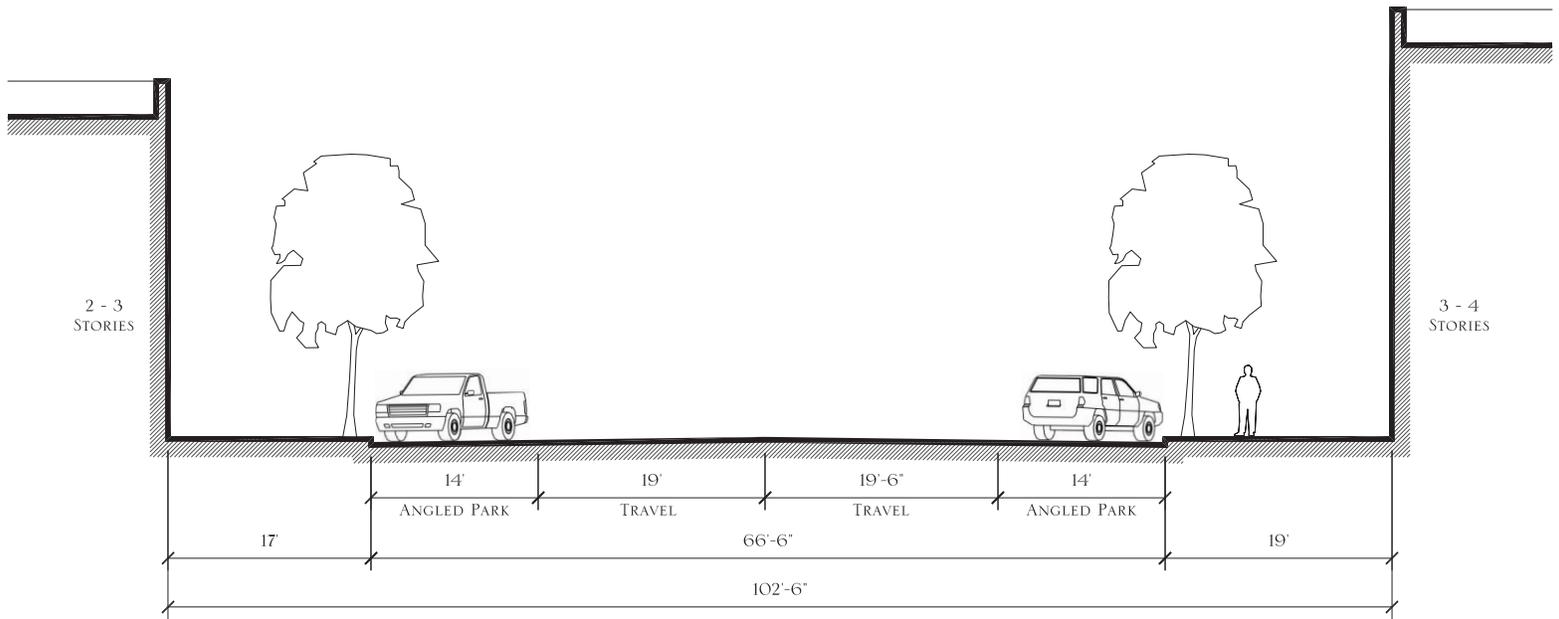


IDENTIFYING FEATURES

1. Town; Consistent street frontage for several blocks along a curving East Main Street, backing up to the Peconic River
2. On-street parallel parking with one driving lane in each direction; turning lane at intersection
3. 12'-6" sidewalks with buffer of concrete or brick, trees, street lamps and benches
4. Parking is located onstreet with midblock lots behind buildings along East Main Street
5. Nearest greenspace is along Peconic River south of East Main Street
6. Three stories at intersection of Peconic and Roanoke but typically one to two stories, attached; Variety wood and masonry buildings with either pitched or parapet with flat roofs; Interesting and varied pedestrian passages to parking along the river and additional shops



SAG HARBOR, NY



Typical Street Section "A-B"

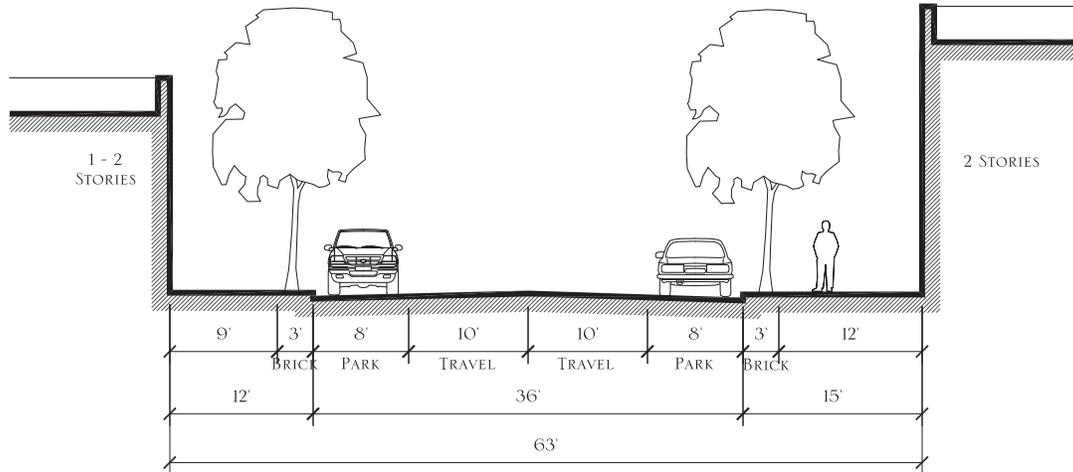


Image: Google Earth, 2016

IDENTIFYING FEATURES

1. Village; Consistent street frontage for several blocks along Main Street and turning the corner onto Bay Street; radial street layout terminating in waterfront
2. Wide street section with angled parking and one driving lane each direction
3. Sidewalks vary in width from 17'-0" to 19'-0" with trees and benches along curb
4. Parking is located onstreet with larger midblock lots behind west side of Main Street
5. Small park along harbor
6. Buildings vary considerably in height, materials, and type; some are attached and three stories especially in the central portion of Main Street, while detached one and two stories; residential buildings that have been repurposed for commercial use at south end of Main Street





Typical Street Section "A-B"

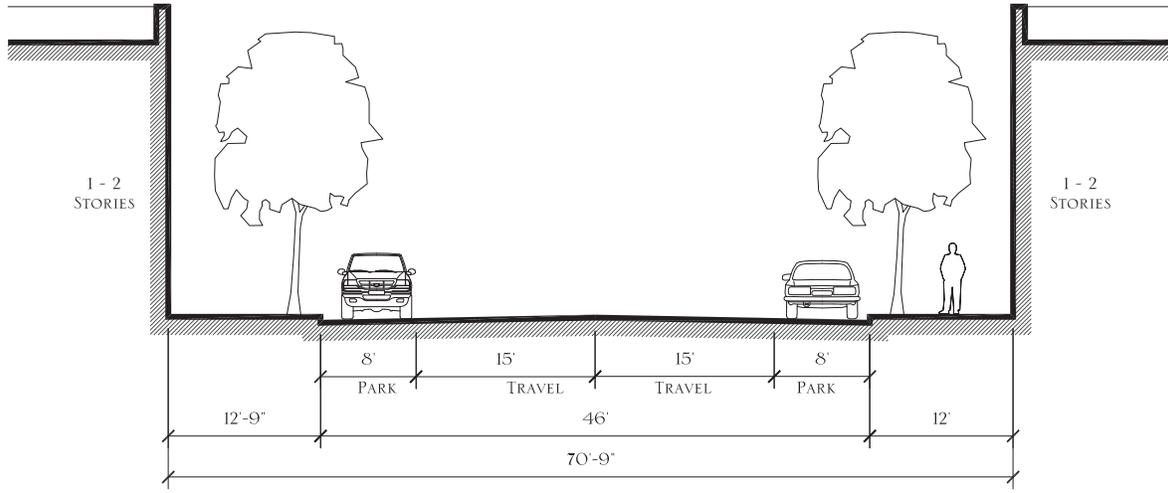


Image: Google Earth, 2016

IDENTIFYING FEATURES

1. Hamlet; Consistent street frontage for about two blocks along Montauk Highway
2. Onstreet parallel parking with one driving lane each direction
3. Large sidewalks with brick buffer, trees, and cobra street lights
4. Parking is located onstreet and midblock behind buildings along the main street (Montauk Highway)
5. Nearest park is baseball field at eastern end
6. Predominantly one story with two stories near intersections, attached; Buildings are mostly parapets with flat roofs and brick or wood cladding; several have elaborate cornices and narrow lots while others are large single use (retail)





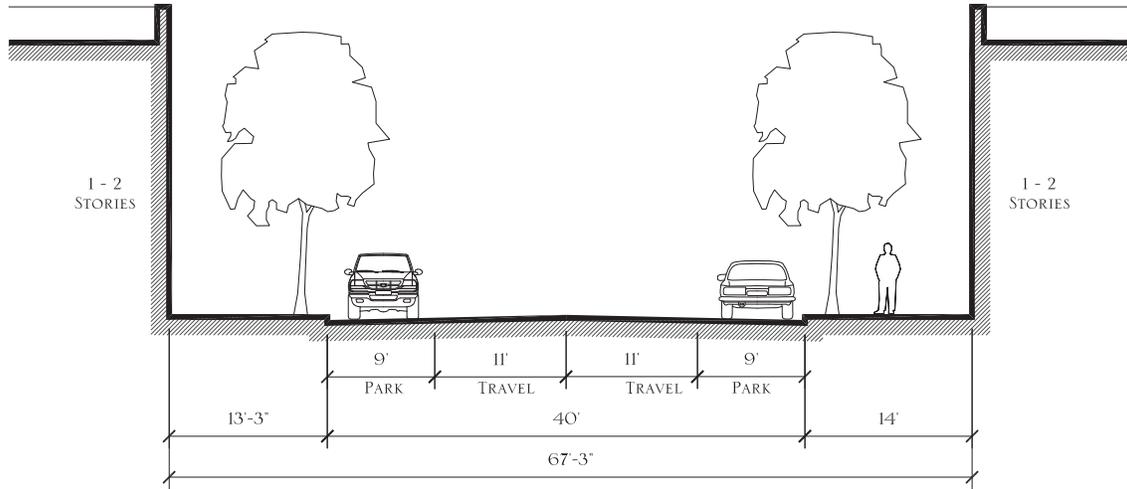
Typical Street Section "A-B"



IDENTIFYING FEATURES

1. Village; consistent street frontage for three blocks along Main Street and continuing along a side street in each direction
2. Main Street has considerably different character from Jobs Lane with regard to turning lane and angled parking with deeper building setbacks; onstreet parking both sides with one driving lane each direction and turning lane at intersections along Main Street
3. Both Main St and Jobs Lane have sidewalks, buffer with trees, benches, and street lamps
4. Parking is located onstreet with several small lots and one larger lot behind west side of Main Street buildings
5. Arts Museum and History Museum have greenspace adjacent to Jobs Lane and via pedestrian passage from Main Street, respectively
6. Predominantly one and two story buildings, attached; Most buildings are masonry or stucco on Main Street, with simple cornices and parapet with flat roofs; wooden buildings along Jobs Lane





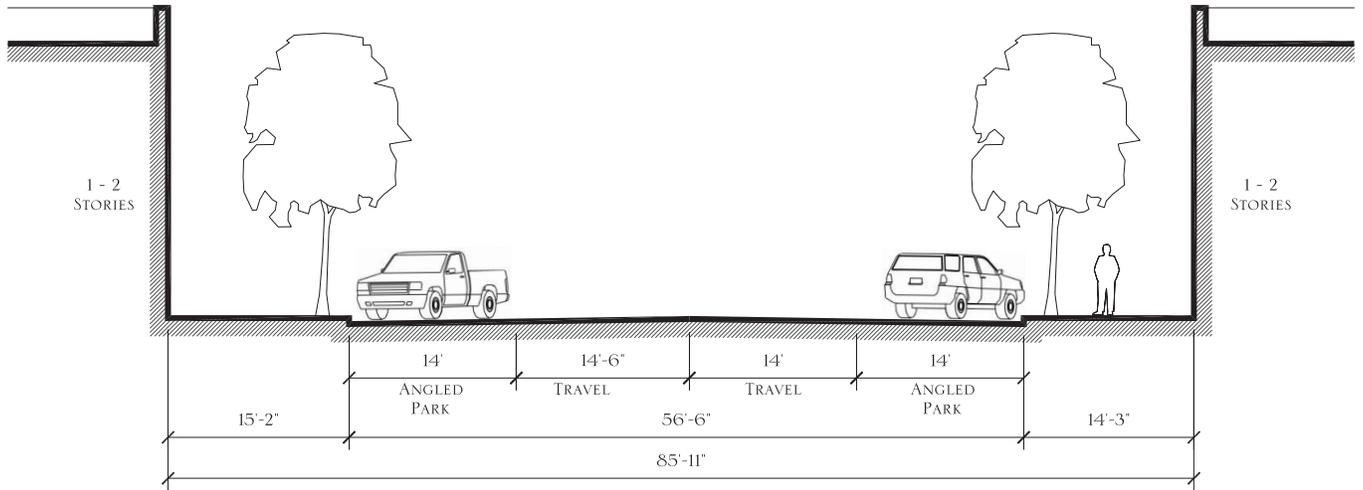
Typical Street Section "A-B"



IDENTIFYING FEATURES

1. Hamlet; Consistent street frontage for about one block
2. Onstreet parallel parking with one driving lane each direction
3. Sidewalk widths vary from 13'-0" to 14'-0"; trees, street lamps, and benches along buffer
4. Parking is located on street with several larger lots adjacent to Main Road away from main intersection
5. Greenspace is located at some buildings set back further from sidewalk; no major park is nearby
6. One and two story buildings; A few attached buildings west of Beckwith Avenue; buildings vary considerably in type, size, massing, style, material, and height





Typical Street Section "A-B"



Image: Google Earth, 2016

IDENTIFYING FEATURES

1. Village; Consistent street frontage for several blocks along a curving Main Street
2. Onstreet angled parking with very large street section; wide drive lanes and fast-moving traffic
3. Sidewalk width varies from 14'-0" to 16'-0", typically, with brick buffer of trees, street lamps, and benches; there are many porches and much outdoor seating
4. Parking is located onstreet with larger municipal lot north of Main Street
5. Some greenspace is located at church near western end of Main Street and a large park across the street
6. Predominantly one and two story buildings; a mix of attached and detached; Architectural details vary but are mostly wood construction with pitched roofs; in-fill porches re-purposed as storefronts; wide street section and low buildings results in a residential feel





2. *Hampton Bays*

Hampton Bays is an established hamlet in the Town of Southampton with a unique history and character all its own.

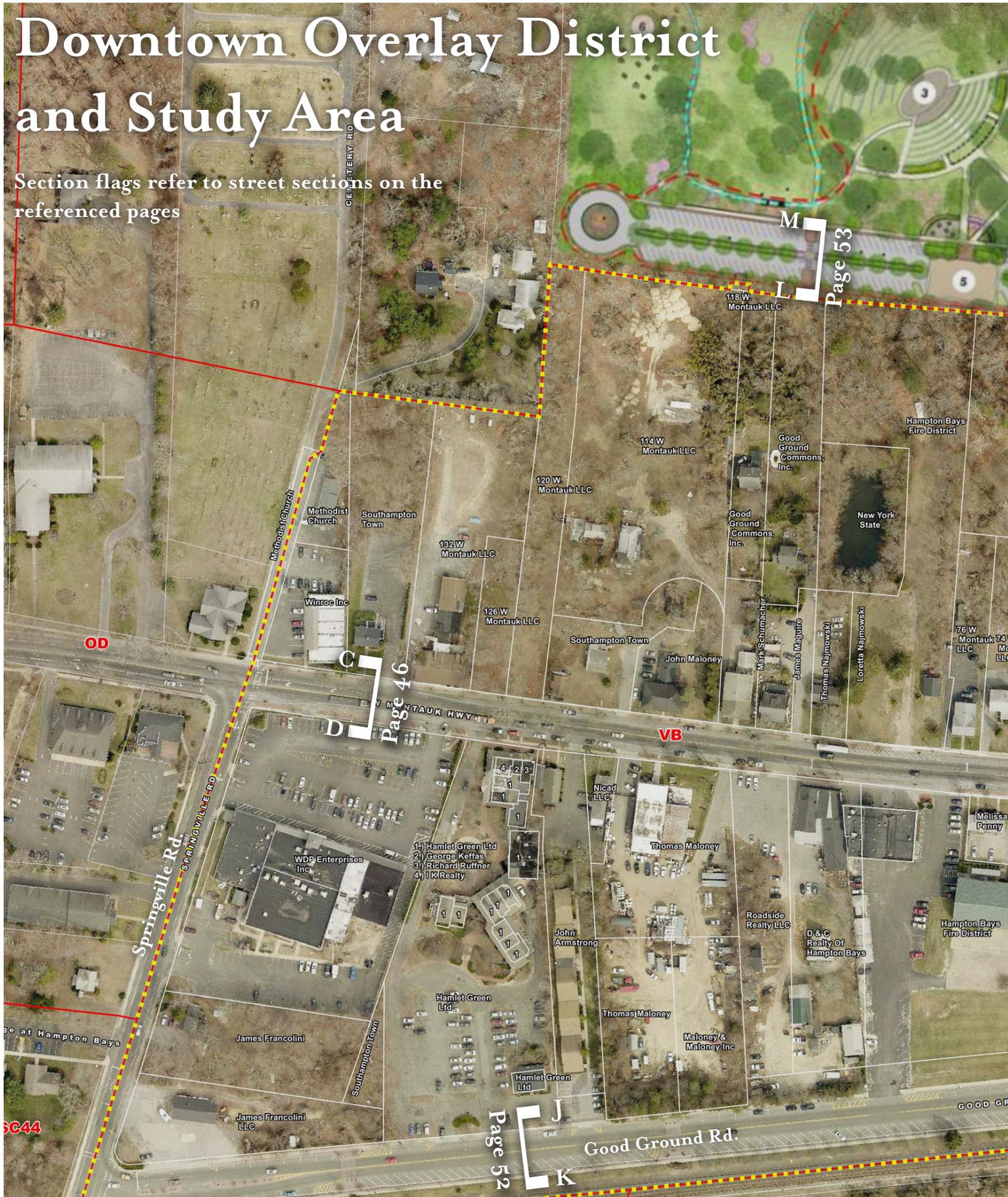
Hampton Bays is surrounded by water: the Great Peconic Bay to the north, Shinnecock and Tiana Bay to the south, and Shinnecock Canal to the east. The Shinnecock Inlet is a popular destination for commercial and sport fishing, and the local port is the second-busiest commercial fishing port in the state. Additionally, the 3.7 miles of publicly owned undeveloped ocean front on the Barrier Islands attracts many visitors and second home owners.

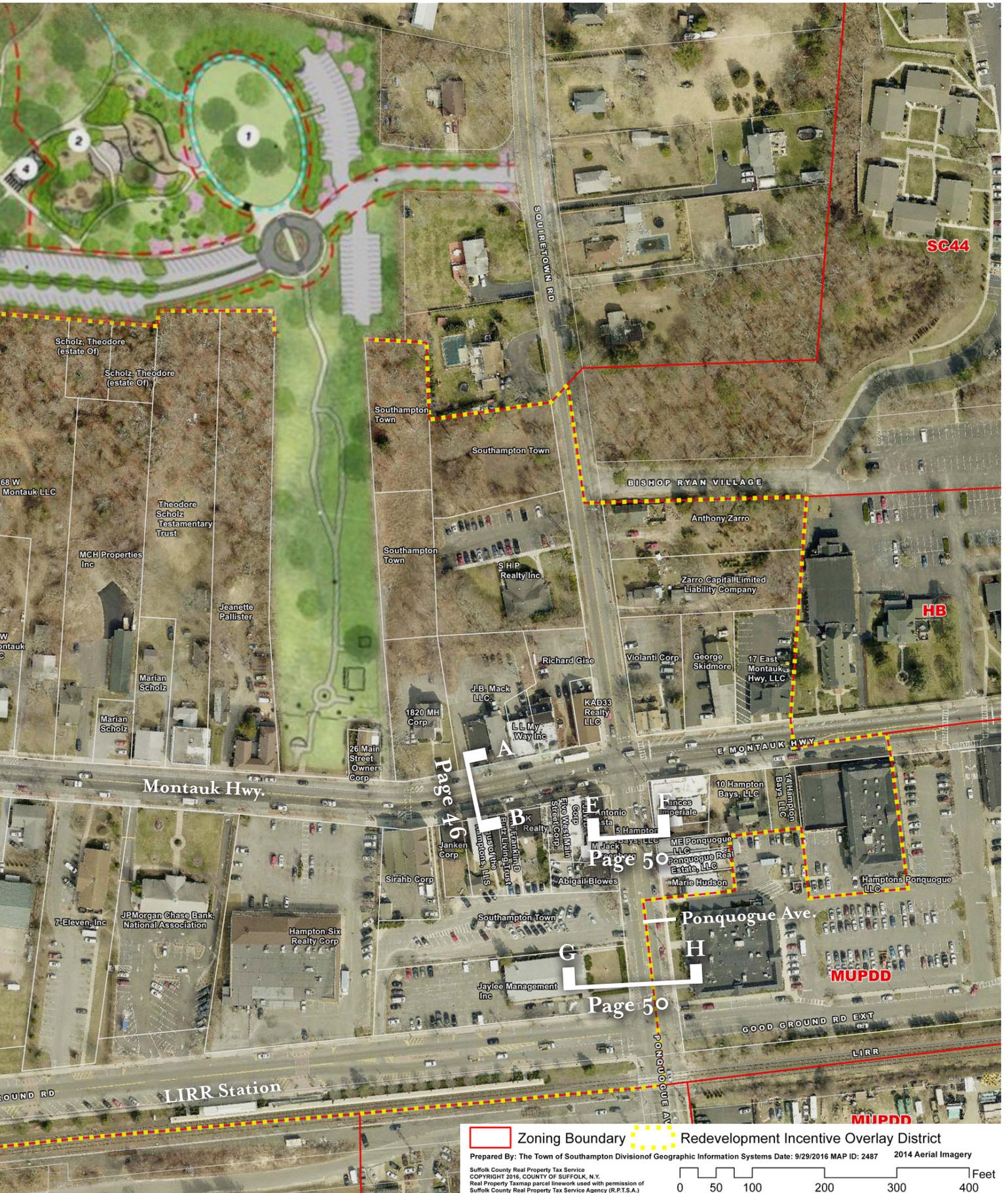
Hampton Bays is easily accessible from New York City via the nearby Sunrise Highway (NYS 27), the Hampton Jitney, and the LIRR which stops in the heart of the hamlet. Hampton Bays is considered to be the Western gateway to “The Hamptons”. However, Montauk Highway, the hamlet’s “main street”, is a major regional connector that brings with it substantial traffic and noise. Given the lack of alternate street connectivity within the area, these traffic issues are only exacerbated.

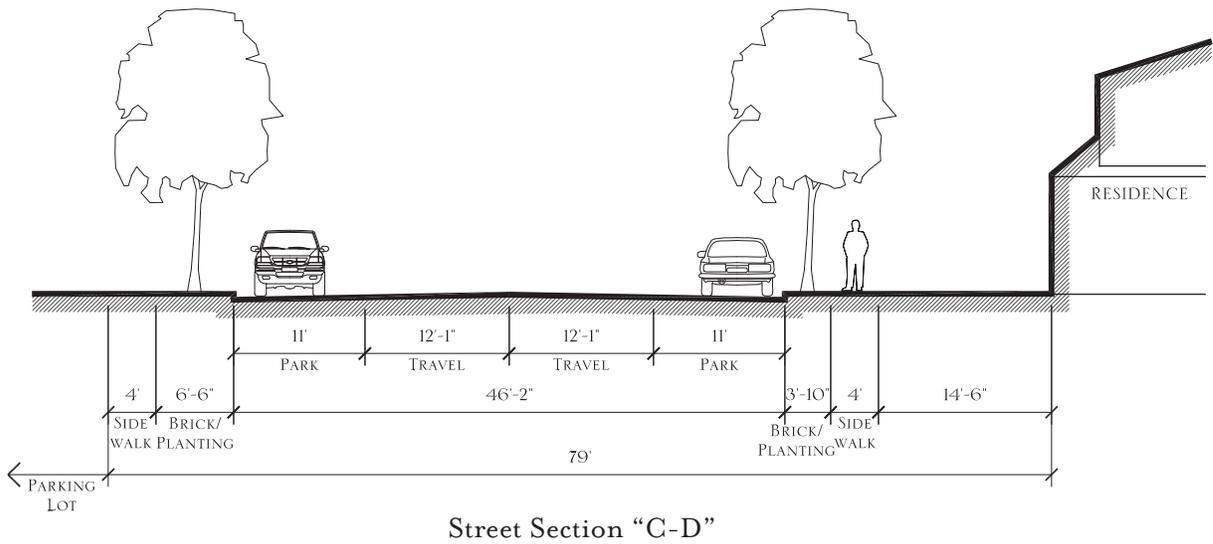
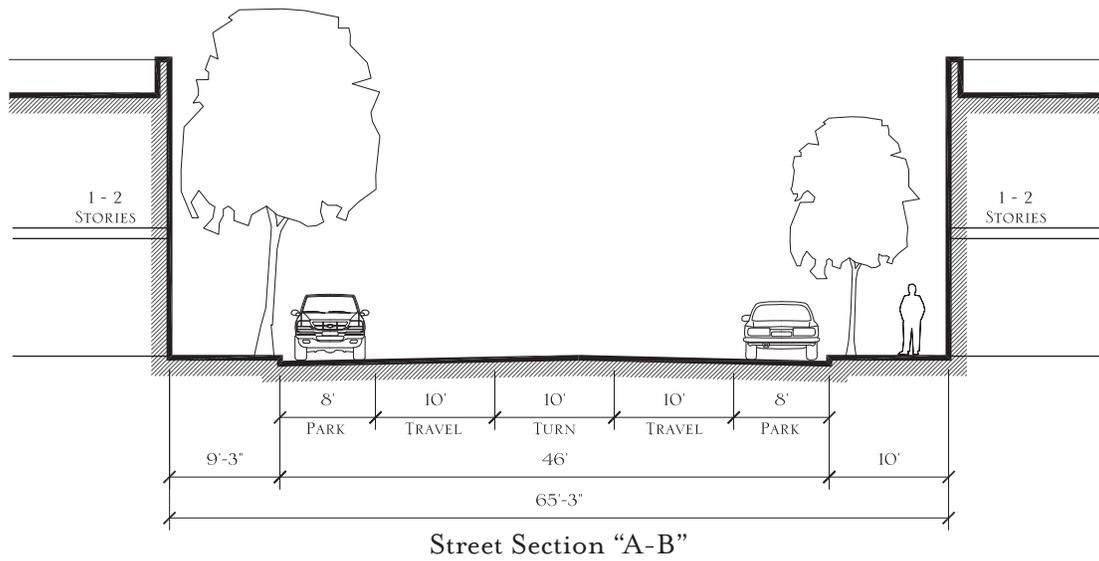
The Downtown Overlay District is situated along Montauk Highway, between Good Ground Park and the LIRR at Good Ground Road. There is a unique opportunity to create a walkable and place-based destination at the heart of the hamlet. However, Hampton Bays must make sure that any new development promotes and strengthens the hamlet’s character and regionally-unique architecture. To that end, the on-site measurements and historical photographs (contained in the following pages) will help to establish a basis and a common language for future development in Hampton Bays.

Downtown Overlay District and Study Area

Section flags refer to street sections on the
referenced pages



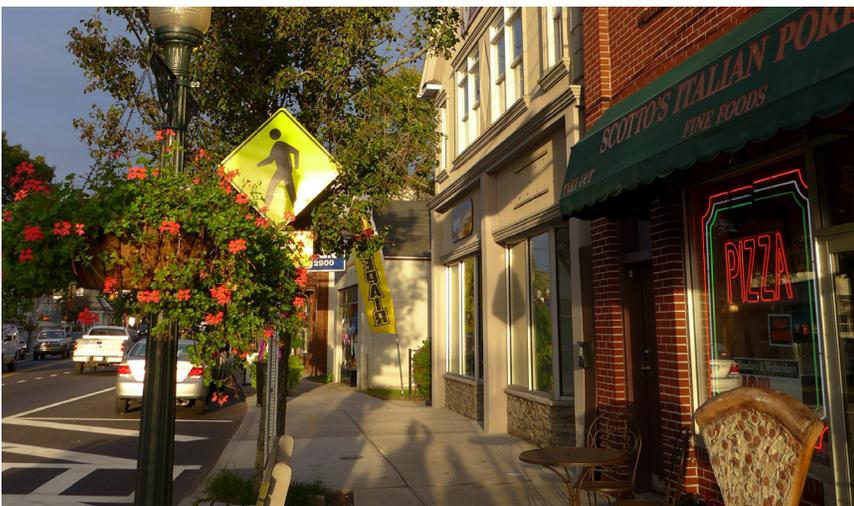




IDENTIFYING FEATURES

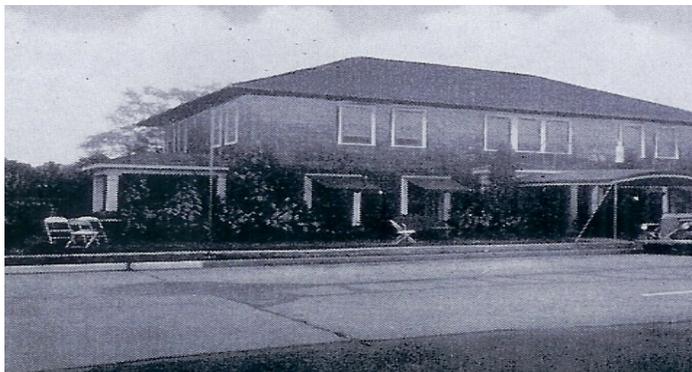
1. Hamlet; lack of consistent street frontage with the exception of an about-two-block core at the intersection of Montauk and Ponquogue; several large single-use suburban-styled commercial establishments to the east and west
2. Wide street section with driving and parking lanes; turning lane near intersection
3. 9'-3" sidewalks along north side of Montauk Highway; width varies along south side; trees, street lamps, and hanging flowers in buffer
4. On-street parallel parking on both sides of street; several parking lots along Montauk Highway, and large midblock lot west of Ponquogue Avenue
5. Good Ground Park one block north of Montauk Highway, with Cannuscio Trail pedestrian connection to Montauk Highway
6. Core intersection of Montauk Highway and Ponquogue Avenue (as depicted in Street Section "A-B") has masonry and wood two-story buildings with pitched roofs; narrower frontage than newer buildings to the west (as depicted in Street Section "C-D"); newer buildings have wider frontage, are set back from the sidewalk

CURRENT PHOTOGRAPHS



HAMPTON BAYS

HISTORICAL PHOTOGRAPHS OF MONTAUK HIGHWAY



HISTORICAL PHOTOGRAPHS OF MONTAUK HIGHWAY

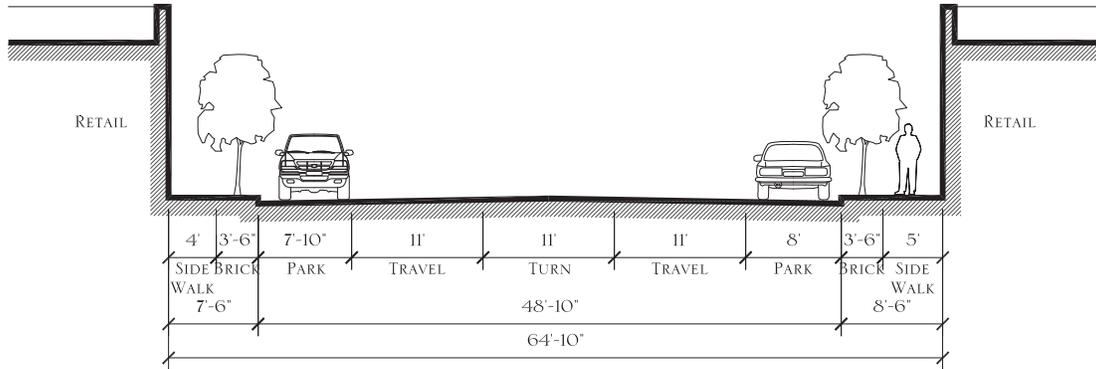


MAIN STREET, Hampton Bays, L. I.

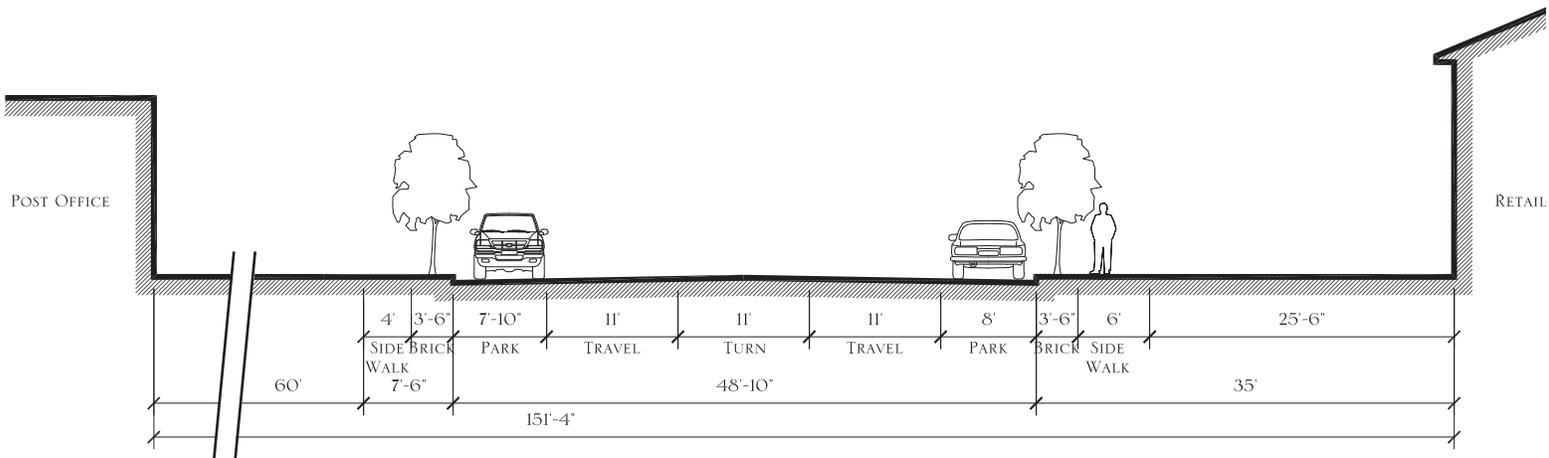
Hand Co



PONQUOGUE STREET SECTIONS



Street Section "E-F"



Street Section "G-H"

IDENTIFYING FEATURES

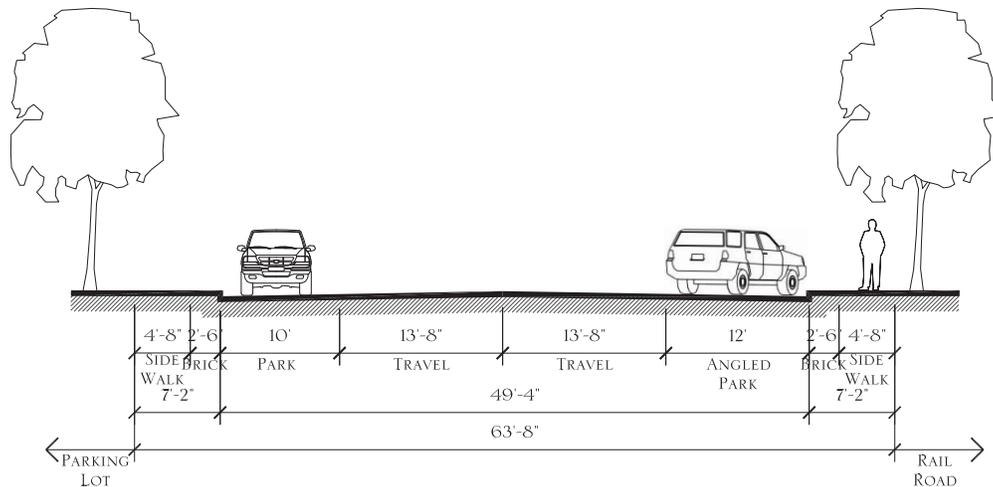
1. Hamlet; lack of consistent street frontage with the exception of an about-two-block core at the intersection of Montauk and Ponquogue; several large single-use suburban-styled commercial establishments to the east and west
2. Two drive lanes, a center turn lane, and parallel parking on either side
3. Narrow sidewalk with consistent brick “planting strip” with trees
4. On-street parallel parking on both sides of street; Midblock parking lot on the west side fronts onto Ponquogue
5. The post office near Good Ground Rd has a substantial setback from Ponquogue (as depicted in Street Section “G-H”).
6. Core intersection of Montauk Highway and Ponquogue Avenue (as depicted in Street Section “E-F”) has masonry and wood one-to-two-story buildings with pitched roofs, buildings on Ponquogue south of Montauk appear to be from the second-half of the 20th century.

CURRENT PHOTOGRAPHS



HAMPTON BAYS

GOOD GROUND ROAD STREET SECTION



Street Section "J-K"

IDENTIFYING FEATURES

1. 2,200-foot block between Ponquogue and Springville.
2. Few buildings front onto Good Ground Rd.; instead, mostly parking lots, undeveloped land, and loading zones are located along the northern edge, and the LIRR tracks define the southern edge.
3. Large street section with oversized driving and parking lanes.
4. Sidewalks on both sides of street with trees being more regular (about 25 feet on center) on the south side.
5. Four mid-block brick crossings appear to be newly constructed and occur at intervals of approximately 500 feet between Ponquogue and Springville.
6. On-street parking on both sides of street: parallel on north side, angled on the south side.

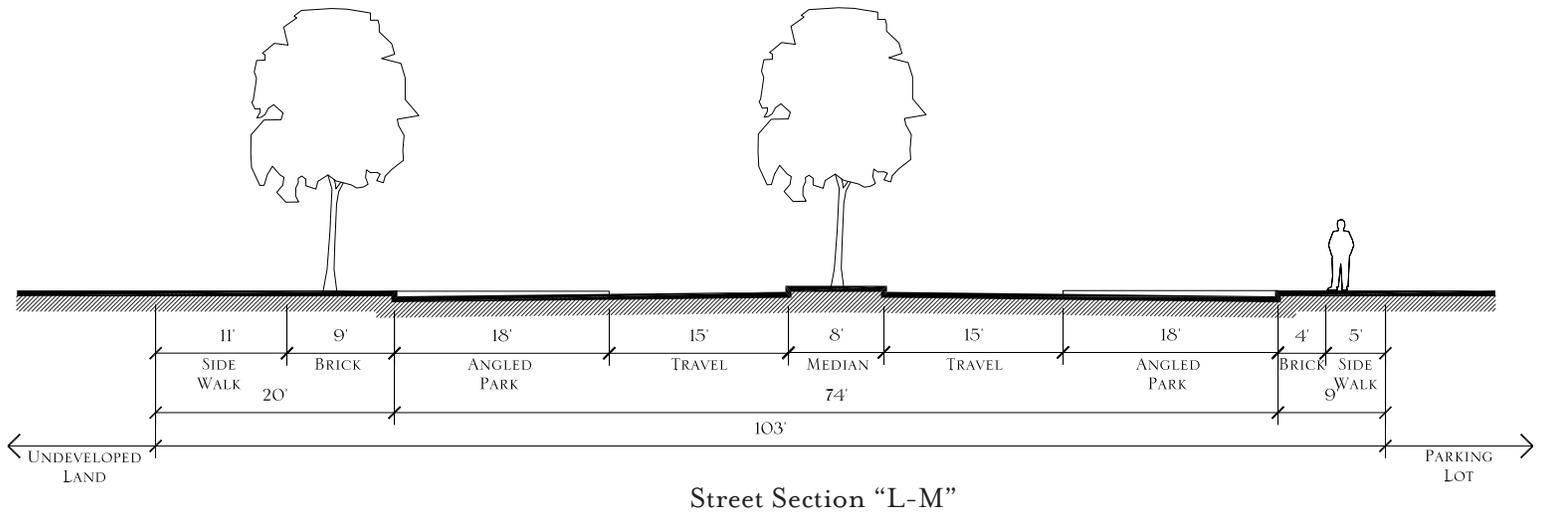
CURRENT PHOTOGRAPH



HISTORIC PHOTOGRAPH



GOOD GROUND PARK STREET SECTION



IDENTIFYING FEATURES

1. 1,350-foot long street; currently no building frontage.
2. Angled parking on either side of large travel lanes with a center planted median.
3. Road terminates into a cul-de-sac. Currently access is limited to Squiretown Rd, but future connections to Montauk are possible.

CURRENT PHOTOGRAPHS



HAMPTON BAYS

GENERAL HISTORICAL PHOTOGRAPHS







3. Figure Ground Studies

FIGURE GROUND STUDIES

When studying built environments, reducing the study to the most basic elements provides the most clear analysis. We do this by creating Figure Ground studies whereby we represent building footprints (the “figures”) and street networks (the “ground”). Additionally, we include primary natural features such as parks and water bodies. In doing so, we channel our thinking to the mass-to-void relationship helping us to identify the “fabric” of the built environment. A visual survey can quickly clue one into the relative densities of each built environment. However, more careful analysis can reveal hard data such as building density per acre, block size comparison, and “façade frequency” (building front per linear foot of street, a measure of activity). This data is meaningful when trying to understand the DNA of a place and how to apply it to new and infill design. Initial analysis of the Hamlets of Long Island relative to Hampton Bays has revealed the following:

1. Hampton Bays has the lowest building density in the entire study (1.1 buildings per acre)
2. Hampton Bays has 4 times the population density as Southampton, but less than half the building density.
3. Hampton Bays’ primary block on main street (between Springville Road and Ponquogue Avenue) is 1,875 linear feet; this is almost twice the length of the second longest main street in the study (Patchogue, at 980 linear feet). Also compare this to the average block size within this study which is about 300 feet by 500 feet.



AMAGANSETT
 BUILDING DENSITY: 1.6/ACRE
 FACADE FREQUENCY: 1/381f



BABYLON
 BUILDING DENSITY: 3.1/ACRE
 FACADE FREQUENCY: 1/391f



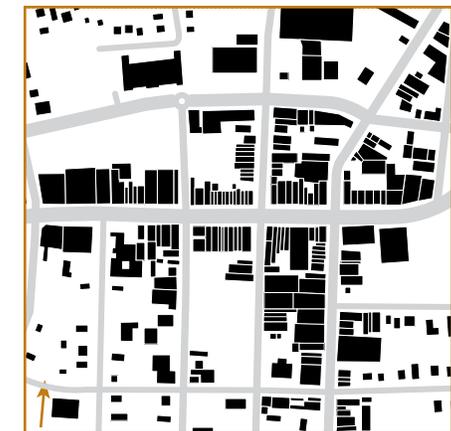
BAY SHORE
 BUILDING DENSITY: 2.2/ACRE
 FACADE FREQUENCY: 1/261f



BRIDGEHAMPTON
 BUILDING DENSITY: 1.7/ACRE
 FACADE FREQUENCY: 1/381f



GREENPORT
 BUILDING DENSITY: 3.5/ACRE
 FACADE FREQUENCY: 1/361f



HUNTINGTON
 BUILDING DENSITY: 3.7/ACRE
 FACADE FREQUENCY: 1/251f



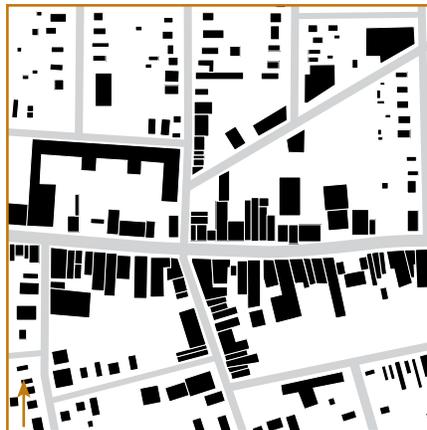
HAMPTON BAYS
BUILDING DENSITY: 1.1/ACRE
FACADE FREQUENCY: 1/50lf



PORT JEFFERSON
BUILDING DENSITY: 2.6/ACRE
FACADE FREQUENCY: 1/35lf



NORTHPORT
BUILDING DENSITY: 2.5/ACRE
FACADE FREQUENCY: 1/25lf



PATCHOGUE
BUILDING DENSITY: 2.9/ACRE
FACADE FREQUENCY: 1/32lf



SAG HARBOR
BUILDING DENSITY: 3.0/ACRE
FACADE FREQUENCY: 1/23lf



SAYVILLE
BUILDING DENSITY: 2.6/ACRE
FACADE FREQUENCY: 1/32lf



SOUTHAMPTON
BUILDING DENSITY: 2.8/ACRE
FACADE FREQUENCY: 1/25lf

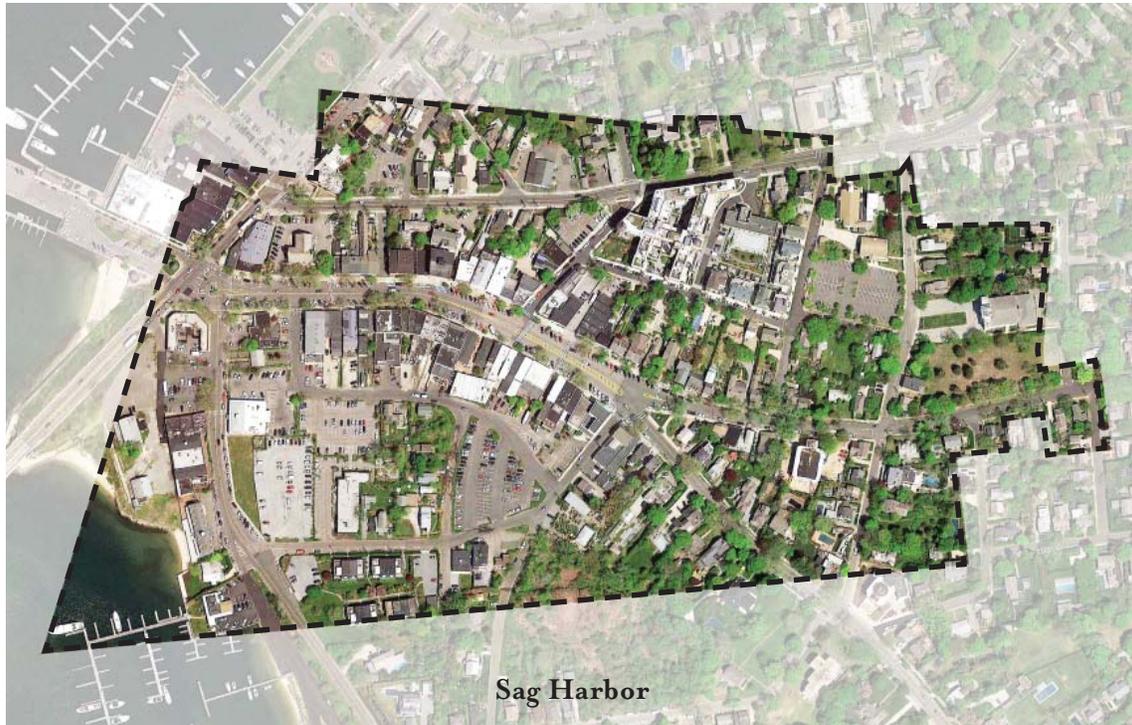


WESTHAMPTON BEACH
BUILDING DENSITY: 1.6/ACRE
FACADE FREQUENCY: 1/45lf

SCALE COMPARISON

Visualizing the relative size and scale of the Downtown Overlay District of Hampton Bays (indicated by the dashed lines below) overlaid on top of other Long Island towns







4. Initial Stakeholder & Public Input

Conversations about Hampton Bays began during our first visit, October 12 and 13, 2016. The Town of Southampton identified key stakeholders to help us develop a framework of understanding of the needs and goals for the Downtown Overlay District. (A list of these stakeholders can be found on page 62.) Additionally, a public meeting was held on November 14, 2016, and an online survey was conducted between November 21 and December 16, 2016, to gather participant's visions for Hampton Bays.

It is important to note that we as consultants are not starting with imported ideas. The citizens are the authors, the idea makers. Our role is to be the editors that ask the right questions and that help formulate the tools for implementing those ideas. Prepared by these initial stakeholder and public inputs, we are now able to assemble questions and materials that will allow us to have meaningful and direct conversations with the general public during an upcoming follow-up public meeting. The following notes and summary came from the initial stakeholder and public meetings.

INITIAL STAKEHOLDER & PUBLIC INPUT

The following comments (on pages 64-65) were gathered from various meetings with stakeholders on October 12-13, 2016.

STAKEHOLDERS

1. Councilwoman Christine Scalera
2. Councilwoman Julie Lofstad
3. Councilman Stan Glinka
4. Deputy Town Supervisor Frank Zappone
5. Janice Landis, Maria Hults representing Hampton Bays Civic Association
6. Gayle Lombardi, Tom Mulrooney representing Citizens Advisory Committee
7. Lars Clemenson representing School District
8. Susan VonFreddi representing Hampton Bays Beautification Association
9. Dot Capuano representing Chamber of Commerce
10. Mark Fasanella representing The Ecological Culture Initiative

EXISTING STRENGTHS OF HAMPTON BAYS AS IDENTIFIED BY STAKEHOLDERS

1. Hard-working, involved, informed, and diverse group of citizens
2. Invested business owners along Main Street
3. The most densely populated area in Hamptons, with largest year-round population
4. Beautiful, undeveloped, public beaches
5. Second largest fishing port in New York State
6. Newly developed Good Ground Park
7. LIRR station adjacent to Main Street
8. Active Beautification Association installing/maintaining flowers and decorations to streetscape
9. Historic structures on Main Street are being restored (Prosper King House and Hat Shop)
10. Restaurants on the water are unique in the Hamptons
11. Fire Department hosts a large number of community events on their land along Main Street
12. Involved student population
13. Primarily, the citizens like Hampton Bays as it is; would just like to fix it up but maintain its character

EXISTING CHALLENGES FOR HAMPTON BAYS AS IDENTIFIED BY STAKEHOLDERS

1. Main Street is not a pleasant place to walk; Narrow sidewalks, numerous parking lots front on to Main Street, with frequent curb cuts, and a busy road.
2. Municipal parking along Good Ground Road is under-utilized because not many business front onto it
3. Existing business owners may not have the means to improve their properties.
4. Restaurants in the downtown area have had a great deal of turn-over
5. Overcrowded housing
6. Poor code enforcement
7. No main draw/anchor tenant to make Main Street a destination
8. The sewer system is operating at its maximum capacity, with no room for expansion
9. There is no continuity in the street wall, form, or language of buildings along the street or from one side of the street to the other
10. Lack of white collar jobs in the area
11. Stormwater runoff and challenges with sanitary have created dead zones in marine ecology; poor water quality
12. Two self-contained population groups that do not inter-mingle
13. The loudest voices of a small group of citizens tend to take over

 OPPORTUNITIES FOR HAMPTON BAYS AS IDENTIFIED BY STAKEHOLDERS

1. Opening of Good Ground Park as a catalyst to draw people to Main Street
2. Existing parking lots, in general, are under-utilized
3. There is a need for affordable housing, and entry level for professionals
4. There is a need for more family-oriented entertainment
5. There is a need to improve stormwater quality
6. A motivated developer has bought a large number of parcels along Main Street
7. There is a great deal of drive-through traffic on Main Street; could be captured as customers
8. Increased LIRR service could make commuting more attractive, and bring more tourism
9. Increased connections to water and beaches via bike trails, community shuttle
10. Redevelopment of Canoe Place Inn may bring more visitors in the future
11. There is a need for mid-level shopping accessible to the population
12. Tax incentives to redevelop motels for increased tourism are being implemented
13. There is a need for a boutique hotel on Main Street to bring more tourism
14. There is a need for public art space: art gallery, theater
15. The current block structure is quite large, so there is an opportunity for new north-south streets or pedestrian cut-throughs to better connect Good Ground Road to Good Ground Park
16. There is a need for an effort to preserve the history that Hampton Bays has
17. There is an opportunity to introduce pocket neighborhoods with small, affordable housing or incubator shops
18. The area is uniquely poised as an ecotourism attraction with the water, parkland, and local colleges; could incorporate farmers markets, demonstration gardens, rain gardens, education components.
19. A business improvement district could be created to help with maintenance of streetscape

 GOALS FOR THE HAMPTON BAYS VILLAGE OVERLAY DISTRICT PATTERN BOOK AS IDENTIFIED BY STAKEHOLDERS

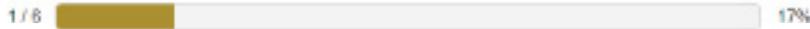
1. Create the framework for the future form-based code
2. Encourage the development of a walkable, thriving, family friendly district
3. Embrace and complement the historic fabric and context of Hampton Bays
4. Create the framework for pedestrian friendly development, attractive storefronts that will allow for a mix of uses over time but provide a consistent and pleasing experience
5. Encourage more consistent setbacks along main street with focused and more usable open space
6. Encourage a consistent architectural and geometric language that will allow for flexibility and diversity over time as the community's needs change

Survey Open November 21 Through December 16, 2016

1,222 Responses

Community Preference Survey
Future Design Of The Hampton Bays Business District

We want your Ideas!



Below is an online survey to get community input for the vision and architectural design for the business district of Hampton Bays. The area of the Downtown Overlay District includes businesses along Main Street and Good Ground Road from Ponquogue Avenue to Springville Road, as well as all the parcels north of Main Street to Good Ground Park.

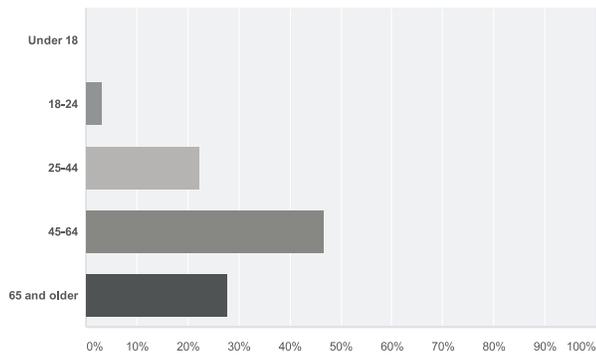
This is your opportunity to help shape the future of the heart of Hampton Bays. Complete the survey below and help design your community!

We look forward to your participation by sharing your preferences and opinions. Please see the Town of Southampton website for updates on future public meetings.

[Click here to enlarge map](#)

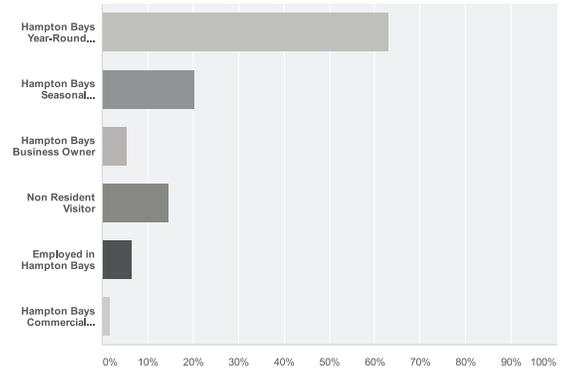


Select your age group.



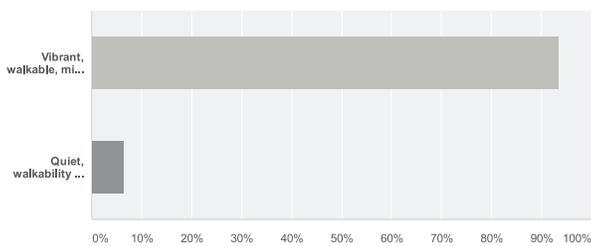
Answer Choices	Responses	Count
Under 18	0.08%	1
18-24	3.06%	37
25-44	22.33%	270
45-64	46.73%	565
65 and older	27.79%	336
Total		1,209

Please select all that apply:



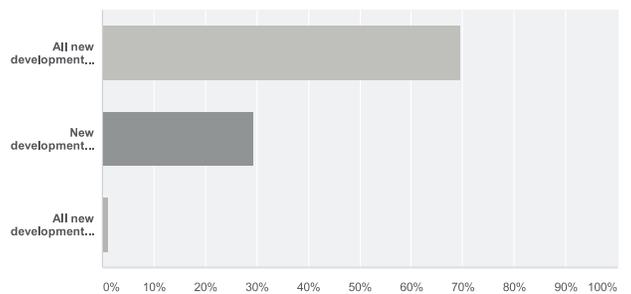
Answer Choices	Responses	Count
Hampton Bays Year-Round Resident	62.95%	796
Hampton Bays Seasonal Resident	20.15%	242
Hampton Bays Business Owner	5.33%	64
Non Resident Visitor	14.65%	176
Employed in Hampton Bays	6.49%	78
Hampton Bays Commercial Owner	1.67%	20
Total Respondents		1,201

Which of the following characteristics most closely matches your future vision for downtown Hampton Bays?



Answer Choices	Responses	Count
Vibrant, walkable, mixed uses, many options for shopping and restaurants, high pedestrian activity	93.50%	619
Quiet, walkability is not important, only a few options for shopping and restaurants, low pedestrian activity	6.50%	43
Total		662

Which of the following statements most closely matches your future vision for the architecture of downtown Hampton Bays?



Answer Choices	Responses	Count
All new development should be of traditional character.	69.62%	463
New development should be a mix of traditional and modern.	29.32%	195
All new development should be modern.	1.05%	7
Total		665

INITIAL STAKEHOLDER & PUBLIC INPUT

What do you like or dislike about the following image?
Feel free to comment on the buildings, sidewalks, and landscaping.



Specific Likes



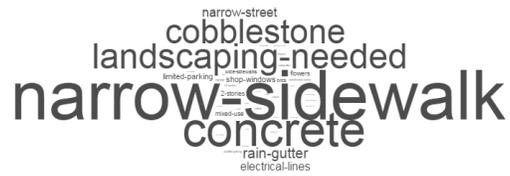
General Positive Words



What do you like or dislike about the following image?
Feel free to comment on the buildings, sidewalks, and landscaping.



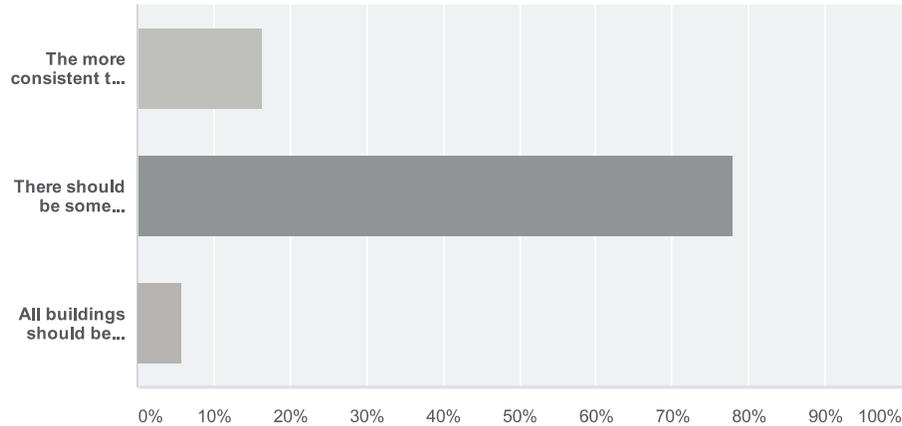
Specific Dislikes



General Negative Words

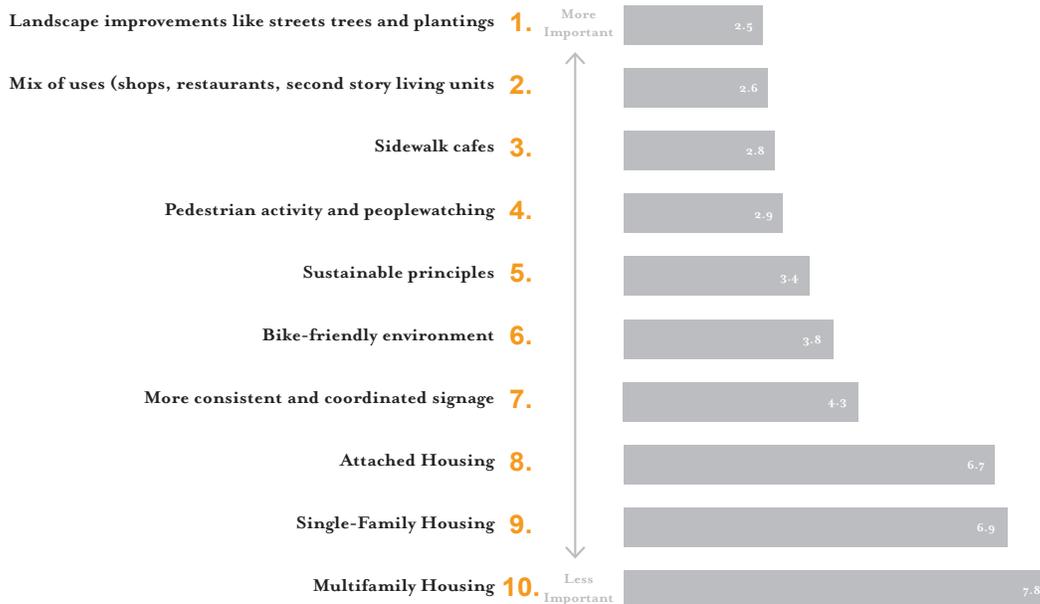


Which of the following statements most closely matches your future vision for the architecture (form and building character) of downtown Hampton bays?



Answer Choices	Responses
The more consistent the architecture (heights, materials, colors, details), the better.	16.28% 105
There should be some consistency in the architecture, but with some variations, to make it appear as though the architecture evolved over time.	77.83% 502
All buildings should be different from one another.	5.89% 38
Total	645

What would you like to see in downtown Hampton Bays? Rank these in order of importance, with 1 being most important and 10 being least important.



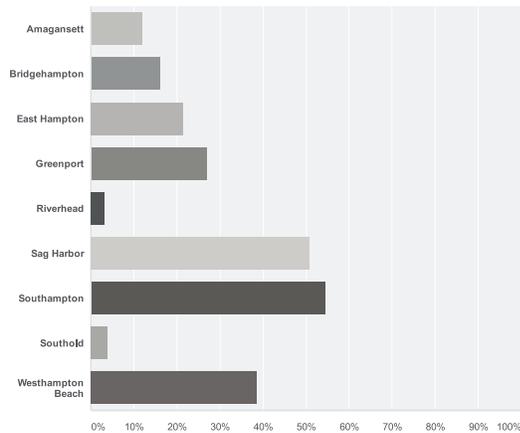
Which of the following storefronts most closely matches your vision for downtown Hampton Bays? Notice the amount of glass, colors, signage, building height and architectural details.

Answer Choices	Responses
	49.69% 321
	5.11% 33
	19.35% 125
	0.31% 2
	0.31% 2
	8.20% 53
	1.24% 8
	13.62% 88
	2.17% 14



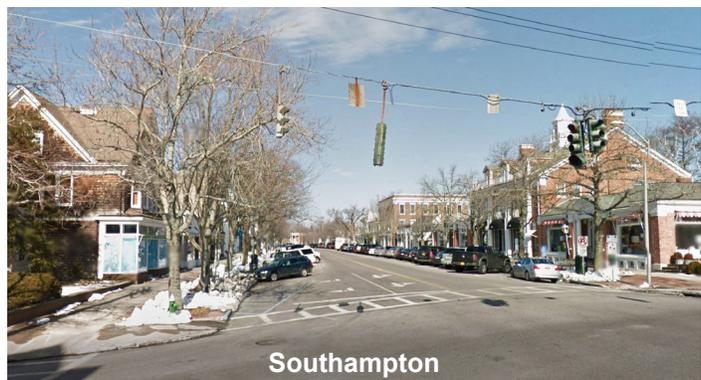
Is there another downtown that embodies the characteristics you would like to see in Hampton Bays? Click all that apply.

Answered: 615 Skipped: 607



Answer Choices	Responses
Amagansett	11.87% 73
Bridgehampton	16.10% 99
East Hampton	21.46% 132
Greenport	26.83% 165
Riverhead	3.41% 21
Sag Harbor	50.89% 313
Southampton	54.47% 335
Southold	4.07% 25
Westhampton Beach	38.70% 238

Total Respondents: 615
Comments (302)



Main Street Form and Scale

Which of the following images most closely match your future vision for downtown Hampton Bays?
For this question, only look at the overall character of the street: building heights, widths, variety, and setback from the street (we will ask you about architecture and details later). Select up to 5 images.

Top 4 images shown below with total vote count.



Main Street Form and Scale

Which of the following images most closely match your future vision for downtown Hampton Bays?
 For this question, only look at the overall character of the street: building heights, widths, variety, and setback from the street (we will ask you about architecture and details later). Select up to 5 images.

Remaining images shown below with total vote count.



Main Street Buildings and Storefronts

Which of the following images most closely match your future vision for downtown Hampton Bays? For this question, notice the architectural style, materials (brick, stucco, wood siding), colors, signage, number of windows and their proportions, and architectural elements like dormers, cornices, and trim details. Select up to 5 images.

Top 4 images shown below with total vote count.



Main Street Buildings and Storefronts

Which of the following images most closely match your future vision for downtown Hampton Bays? For this question, notice the architectural style, materials (brick, stucco, wood siding), colors, signage, number of windows and their proportions, and architectural elements like dormers, cornices, and trim details. Select up to 5 images.

Remaining images shown below with total vote count.



Streetscape and Landscape

Which of the following images most closely match your future vision for downtown Hampton Bays? For this question, notice the plantings, sidewalk widths and materials, outdoor furniture, parks, and street trees. Select up to 3 images.

Top 4 images shown below with total vote count.



Streetscape and Landscape

Which of the following images most closely match your future vision for downtown Hampton Bays? For this question, notice the plantings, sidewalk widths and materials, outdoor furniture, parks, and street trees. Select up to 3 images.

Remaining images shown below with total vote count.





5. Review of Past Planning Initiatives & Objectives for the Pattern Book

Historical Concepts has reviewed applicable past planning initiatives and studies as identified for them by the Town of Southampton. The following section contains major concepts from those past planning initiatives, and recommendations that Historical Concepts has formed in response to those major concepts. Historical Concepts's responses have been informed by the Data Gathering Process.

This section also contains the objectives that Historical Concepts has outlined for the Pattern Book, based on their evaluation of the public's input and preferences.

PAST PLANNING INITIATIVES

HAMPTON BAYS CORRIDOR STRATEGIC PLAN, JULY 2010 BY: HUTTON ASSOCIATES

1. The concept of a “New North Main Street” presented in the Corridor Strategic Plan is thought to bring new infill adding commercial activity and critical mass for economic vitality. (Ref: pp 25, 37; also Hampton Bays Corridor Strategic Plan & Cumulative Impact of Buildout Study – SEQR Findings Statement, September 2013 p. 33)
 - We agree that an east-west road should exist along the southern edge of Good Ground Park. However, our opinion is that requiring commercial along this new road will likely draw infill development opportunities and renovation investment away from Montauk Highway, thus creating less critical mass in the historic core. Additionally, it has been proven that a street with one side of commercial uses is often unsuccessful.
 - Our recommendation is to allow a mix of uses, to include hospitality and residential, fronting the park and focus commercial development efforts within the existing historic core.
2. Although outside of the Downtown Overlay District, we advocate the design solution for the area referred to as the “Asphalt Jungle” (a paved, treeless area in which perpendicular parking in front of buildings is accessed directly from the roadway). (Ref: p 42)
 - It presents a more orderly and safe parking arrangement while screening the parking from Montauk Hwy.
3. It is documented that the residents wish to have more hamlet greens. (Ref: pp 11, 27, 39) At the same, it was stated that low density development is undesirable, specially calling out the church across from the King Kullen development. (Ref: p 52) A similar parallel could be made with the lawn in front of the Chase Bank.
 - Instead of adding more gaps in the streetscape by creating additional greens, we would recommend trying to capitalize on the greens that are already in place, established and maintained. This could be a partnership with a property owner to establish a “more public” aesthetic or design for the existing spaces; or, it could simply be helping to establish one of these greens as a site for an annual event.
4. An Economic Development Study was recommended by the Corridor Strategic Plan and we agree that this is a vital part of planning a better Hampton Bays.
 - Such a study could identify the types of uses that are not currently being supplied in the region, and could provide a map for success in seeking new development programs.
5. The Hampton Bays Hamlet Mixed Use Planned Development District Study, October 2002 highlighted several “development themes” that were generated by attendees of the community design charrette. These included a senior housing complex and hotel conference center.
 - While we agree these are uses that make a lot of sense in Hampton Bays, only an economic development study can provide more surety. (Also see note in Hampton Bays Corridor Strategic Plan & Cumulative Impact of Buildout Study – SEQR Findings Statement, September 2013 regarding Stanton & Legett study)

HAMPTON BAYS CORRIDOR STRATEGIC PLAN & CUMULATIVE IMPACT OF BUILDOUT STUDY –
SEQR FINDINGS STATEMENT, SEPTEMBER 2013 BY: CASHIN ASSOCIATES

1. Table 1 references a study by Stanton & Legett for HBSD and indicates a trend toward more senior housing in the Hamlet. Maintaining or promoting resort uses, senior housing, second homes and condos, and cooperatives, and smaller dwelling units would mitigate impacts on schools. (Ref: p 6)
 - Most of these residential unit types would be good fit for second floor and even some ground floor uses within the Downtown Overlay District to create a more safe, active and inviting downtown.
 - We also feel that concerns about illegal crowding in existing single family houses (Ref: p 7) could be partially alleviated by providing additional, smaller primary residential units.
2. Active recreation facilities are mentioned (Ref: p 7) in the SEQR Findings Statement as well as in the Hampton Bays Hamlet Center Strategy, November 1999 BY: Hutton Associates. (Ref: p 36)
 - We feel that such a use would best be located fronting Good Ground Park. This would create a significant ground floor use and be more appropriate than locating commercial on the park. It could also help to bring more pedestrian traffic to the park.
3. Table 2 documents a recommendation to investigate the feasibility of burying electrical utilities and eliminating telephone poles in the Hamlet to improve aesthetic qualities. All new utilities should be installed underground. (Ref: p 16)
 - We fully support this recommendation and advise the Town to require this as a part of any new development plans.
4. Table 2 documents a recommendation to create a façade improvement program to stimulate upgrades in the hamlet center through loans, grants, or tax abatements. (Ref: p 17)
 - This is an important initiative that should be developed. National programs such as Main Street America should be investigated, as well as local organizations.
 - Rehabilitating and renovating existing buildings is a critical part of maintaining the character of downtown, especially as new infill is built.
5. Table 2 documents a recommendation to promote/require the use of native plant or ornamental species well-adapted to area and site conditions. (Ref: p 23)
 - The use of native plantings (with careful selection) could also be supportive of treating and filtering rain water, which we've heard in discussions with the public is a region-wide concern.
 - Additionally, the correct application of a plant pallet tuned to the local climate (USDA Cold Hardiness Zone 6B) can also be a part of a sustainable approach to landscaping with regard to such concerns as watering requirements, reduced maintenance, and pest control.
6. Table 2 documents a recommendation to work with the LIRR to provide more frequent and locally-oriented service. (Ref: p 36)
 - This aligns with comments we heard from the public as well. Certainly, the LIRR is a great and unique asset to Hampton Bays and should be capitalized on in any way possible. Housing within walking distance to the LIRR stop would make it a more appealing option for commuters, which aligns with a recommendation on p 38.

PAST PLANNING INITIATIVES

HAMPTON BAYS HAMLET MIXED USE PLANNED DEVELOPMENT DISTRICT STUDY,
OCTOBER 2002 BY: DEPARTMENT OF LAND MANAGEMENT

- i. Goals for the secondary study area (the area very close to Downtown Overlay District study area) included: (Ref: pp 27, 28) municipal parking areas behind businesses on the north side of main street with pedestrian access routes; mixed-use infill (commercial/retail with residential above); maintain and enhance the mixed-use character and density of the community.
 - We fully embrace these goals and aim to create pattern book that will help them become a reality. Two of these goals involve “mixed-use” which we believe is very important to the vitality of downtown.
 - An important use that is lacking within the Downtown Overlay District is residential. An increase in residential uses in both ground floor and second floor spaces will provide more 24-hour “eyes on the street”, reduce the amount of car trips around the District and will provide smaller, more affordable units to add variety to the current housing offerings.

HAMPTON BAYS HAMLET CENTER STRATEGY,
NOVEMBER 1999 BY: HUTTON ASSOCIATES

1. We support the transportation proposal of redesigning the geometry of Route 24 and Montauk Hwy intersection (moving it further east and creating a triangular median for signage or art).
 - This shifts the intersection away from the diner building. In the near term this would give drivers the terminating view of forest. It also pulls the Route 24 further from the Macys parking lot, providing an opportunity for more landscape buffering.
 - The most important reason for this redesign is to set up a future extension southwards to be able to connect with a future westward extension of Good Ground Road. This is our recommendation for resolving the hamlet traffic challenges – by using Good Ground Road as a bypass (for through-traffic and for those intending to travel directly to the beach). This is the major difference in corridor recommendations of our regulating plan and the Hamlet Center Strategy. (It is also counter to Town Board recommendations in the Hampton Bays Corridor
 - We also recommend that the extension of Good Ground Road westward beyond Springville Road be a public road rather than cross-access drives between defined parking areas as considered in the Hamlet Center Strategy.
 - In the event that ROW for the extension of Route 24 (south) and Good Ground Road (west) is not attainable, we support the proposal of widening Montauk Hwy for two eastbound lanes from Route 24 to Springville. This would provide better capacity to the point of the north-south connector road which, we feel, would greatly disperse traffic around main street.
2. We agree with the identified Core Area (“activity center including post office, rail station, and dense cluster of shops and restaurants”) being around Ponquogue Ave/Montauk Highway/Good Ground Road.
 - However, we could see it expanded to include future redevelopment sites that continue a similar density. This would extend a similar character and density westward on Montauk, ending at the 7 Eleven site, just west of the Chase Bank.
3. The desire for additional greens seems out of place considering there is more green space along main street in Hampton Bays than in most other hamlets.
 - Our recommendation is to better frame and utilize the greens that are present. If current greens are private ownership, then some commitment would have to be made to make them semi-public in perpetuity. We also feel that festivals and markets would best be associated with Good Ground Park or the green and promenade that currently lead to the Park. This brings those functions closer to parking supply and keeps foot traffic closer to the core area.
 - We agree with the idea of “Montauk Green” but feel the proposed location would be redundant to the lawn in front of the Chase Bank, and would make for too large of a greenspace along Montauk Hwy. We propose that the Chase Bank lawn become the civic green with new development on east and west to frame it. This may require a public-private arrangement with the Chase Bank.
 - We believe that the addition of planting material along the public-realm could also address the desire of the public to see more greenery. The inclusion of street trees and planting strips in future streetscape improvements or developments, and encouraging shop owners to install planters and flower boxes along their storefronts could help to achieve this.

OBJECTIVES FOR THE PATTERN BOOK

The goal of the Pattern Book for the Hampton Bays Downtown Overlay District is to synthesize the past planning initiatives and studies, the local and historical patterns of development, and architectural expressions in order to provide a blueprint for future development. In order to meet this goal, Historical Concepts has determined that the Pattern Book must:

- I. Define the appropriate scope and intensity of the Downtown Overlay District relative to other towns/villages/hamlets in the area while encouraging development that will maintain and reinforce the character of Hampton Bays.
 - The development patterns must provide a flexible framework which would allow Hampton Bays to adapt to future unknown market conditions, and support more or less development in the future.
 - Large-scale development may be more economically viable, but infill development and façade improvements will need to be encouraged to achieve vibrant, walk-able area as desired by survey participants
 - In order to be sustainable, development must result in a place that is treasured, loved and maintained for generations to come
 - Focusing higher intensity development in the Downtown Overlay District will allow the residential character of the remainder of the Hamlet to be maintained while addressing the demands of population growth and economic development. This approach should also alleviate the development pressures on open space and park land located throughout the Hamlet.
 - Focusing higher intensity development in the Downtown Overlay District is responsible and an efficient use of resources as the transportation infrastructure is already in place to support it (NYS 27, LIRR, Hampton Jitney, Montauk Highway)
 - Instead of adding more gaps in the streetscape by creating additional greens, we would recommend trying to capitalize on the greens that are already in place, established and maintained. This could be a partnership with a property owner to establish a “more public” aesthetic or design for the existing spaces; or, it could simply be helping to establish one of these greens as a site for an annual event.
 - Development should mitigate potential storm water runoff and utilize native plants where possible

2. Define the design elements of the public realm of the Downtown Overlay District in a manner consistent with the preferences of the survey respondents
 - Encourage the use of wide sidewalks, street trees and plantings, large store fronts, and consistent signage
 - Shape the public realm of a future framework of new connecting streets that will provide a comfortable pedestrian experience to complement existing Montauk Highway
 - Recommend reducing required parking minimums
 - Encourage parking to be located behind buildings that front the street, or appropriately screened in order to ensure the comfort of the pedestrian
 - Encourage the use of signage, lighting, and landscaping that will contribute to safety and positive experience of users of the public realm

3. Encourage a mix of uses throughout the Downtown Overlay District as necessary in order to be a vibrant, pedestrian-friendly area that attracts residents and visitors alike
 - A mix of uses contributes to a safe and welcoming streetscape by ensuring that there are “eyes on the street” during more times of the day
 - Residential units within the District are necessary in order to support retail, restaurant and service-based establishments
 - A mix of business, residential, retail, hospitality, office, and educational uses would reduce vehicle trips and could provide foot traffic at different times of the day
 - Demand for certain uses will change over time, so the focus should be on the physical form of future development that will reinforce character desired by survey participants

4. Define the physical form/building massing that future development should take in order to generate an inviting, traditional main street with “small-town-charm”
 - Maintain 1-2.5 story buildings along Montauk Highway to reinforce the existing scale and character of that area
 - Revise the maximum allowable building height to govern the eave/cornice height instead of the roof ridge in order to encourage the use of sloped roofs and dormers that will help to create a more residential character as desired by survey participants
 - Maintain maximum allowable height of 35’ but allow up to 3.5 stories along future connecting streets in order to allow new development that is economically viable in the modern economy.
 - Allow buildings of up to 35’ and 3.5 stories along the LIRR corridor in order to encourage transit-oriented development
 - Encourage a mix of building heights to avoid monotonous streetscapes
 - Encourage smaller building footprints to keep interest and variety along street front; allow larger buildings provided they are articulated like a series of smaller frontages.
 - Define the size and placement of openings along street frontage in order to contribute to the feeling of a welcoming and safe street.

5. Define the physical form/architectural style and details that future development should take in order to generate an inviting, traditional main street with “small-town-charm”
 - Encourage the use of natural materials, traditional detailing, and muted colors consistent with the preferred imagery
 - Encourage architectural designs that are consistent with the historical patterns in the area as these details and forms have developed locally and withstood the test of time and climate.
 - Taste and beauty may be subjective to some degree, but the survey participants expressed a preference for traditional forms and detailing: these items will help to contribute to the safety and well-being of people using the public realm in the Overlay District.

Drawings contained herein are conceptual in nature and are not released for construction. Maps Copyright 2016 by Google Earth. Accessed October 2016. 2017 Copyright by Historical Concepts with all rights reserved subject to the terms and conditions of the agreement between Client and Historical Concepts.

430 PRIME POINT SUITE 103
PEACHTREE CITY, GA
770.487.8041
HISTORICALCONCEPTS.COM©2017
HISTORICALCONCEPTS,LLC